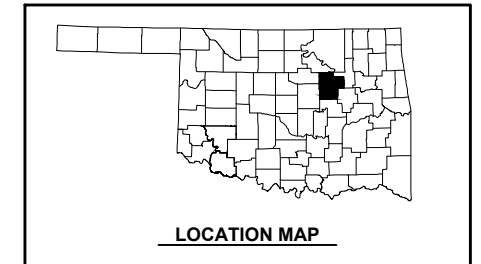




OKLAHOMA TURNPIKE AUTHORITY

TURNER TURNPIKE

PAVEMENT REHABILITATION Milepost 194.0 to 200.0, Length 6.0 Miles CONTRACT NO. T-MC-114 CREEK COUNTY, OKLAHOMA



NOT FOR CONSTRUCTION

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INDEX OF SHEETS

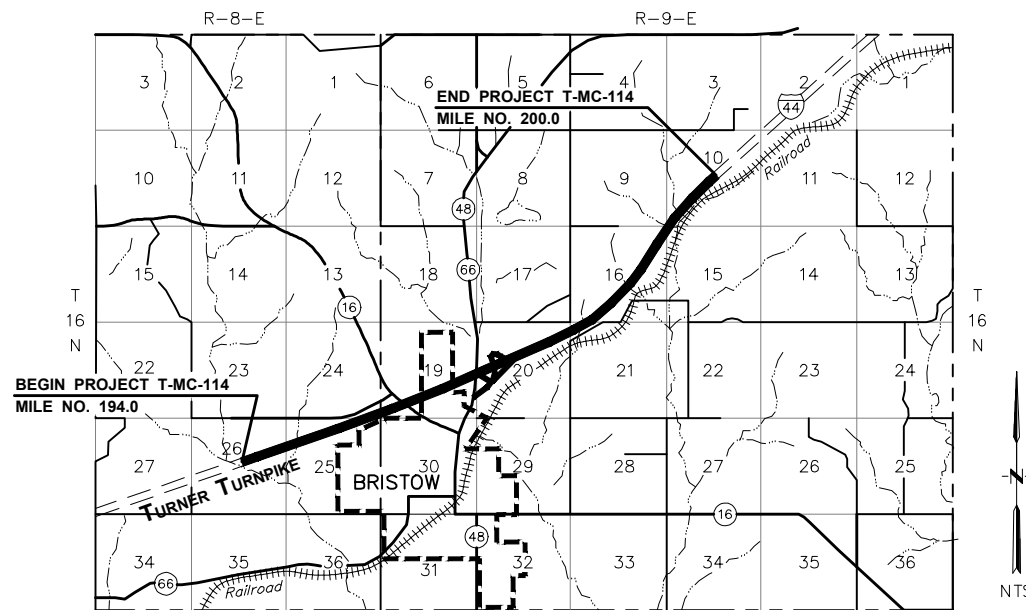
SHEET No.	DESCRIPTION
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3.	Typical Sections
4.	Pay Quantities and Notes
5.	General Construction Notes and Median Barrier Detail
6.	Summaries of Guardrail, Repair and Surfacing
7.	Summaries of Drainage Structures and Signing
8.	Storm Water Management Plan
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10.	Guardrail Details
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TSC2-3-0	PM4-1-01	GHW1-1-00	TCS13-1-00
CLB-1-1	PM5-1-00	GHW2-1-00	TCS14-1-00
PDT-1-1	PM6-1-00	RS2-1-00	TCS15-1-00
	WSD1-1-00	TCS1-1-01	TCS17-1-00
	WSD3-1-00	TCS2-1-00	TCS18-1-01
	SBS1-1-00	TCS3-1-01	TCS19-1-01
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	GMS2-1-00	TCS6-1-02	TCS22-1-00
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	SSA1-1-00	TCS8-1-00	TCS25-1-00
	THRI-1-00	TCS9-1-01	
	ET31-1-00	TCS10-1-00	

DESIGN DATA

ADT 2011	25,850
ADT 2021	28,440
Directional Distribution	0.50
Lane Distribution	0.80
Total Truck	23%
Flex Esals (10 year)	.19.2 million
Flex Esals (40 year)	.89.8 million
Design Speed	75 mph



ROADWAY PROJECT LENGTH 30,635 FEET 5.802 MILES
PCC PAVEMENT AND
BRIDGE EXCEPTION LENGTH 1,410 FEET 0.267 MILES
TOTAL LENGTH 32,045 FEET 6.069 MILES

NOTE: This Design is Specifically prepared for use at the location shown. Use in any other manner exceeds the intended purpose of these drawings and any accompanying specifications.

PREPARED BY
CRAIG & KEITHLINE, INC.
Consulting Engineers
Tulsa, Oklahoma



By _____ Date _____
Kevin A. Kriewall, P.E.
Oklahoma Reg. No. 14850

Craig & Keithline, Inc.
Oklahoma Certificate of Authorization No. 1216
Renewal Date - June 30, 2014

REVIEWED BY
SAIC

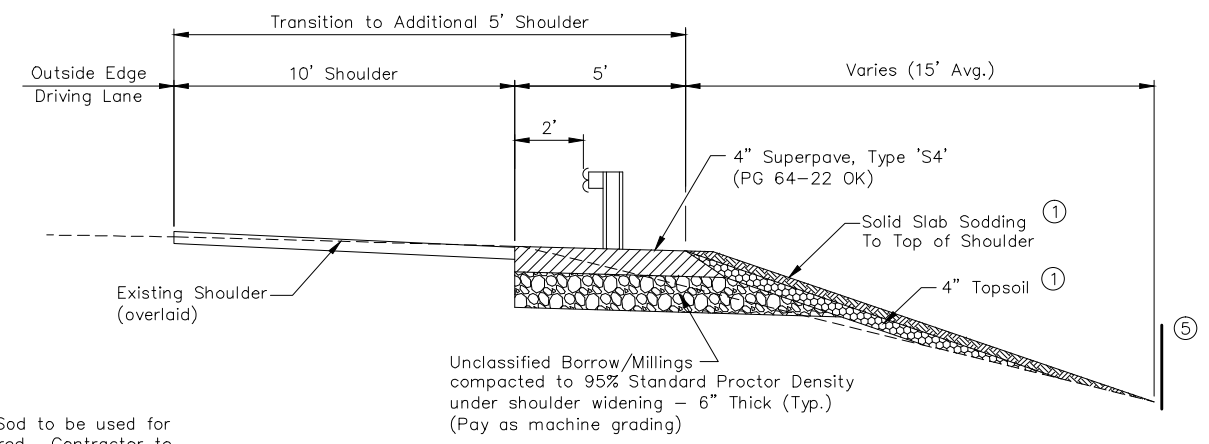
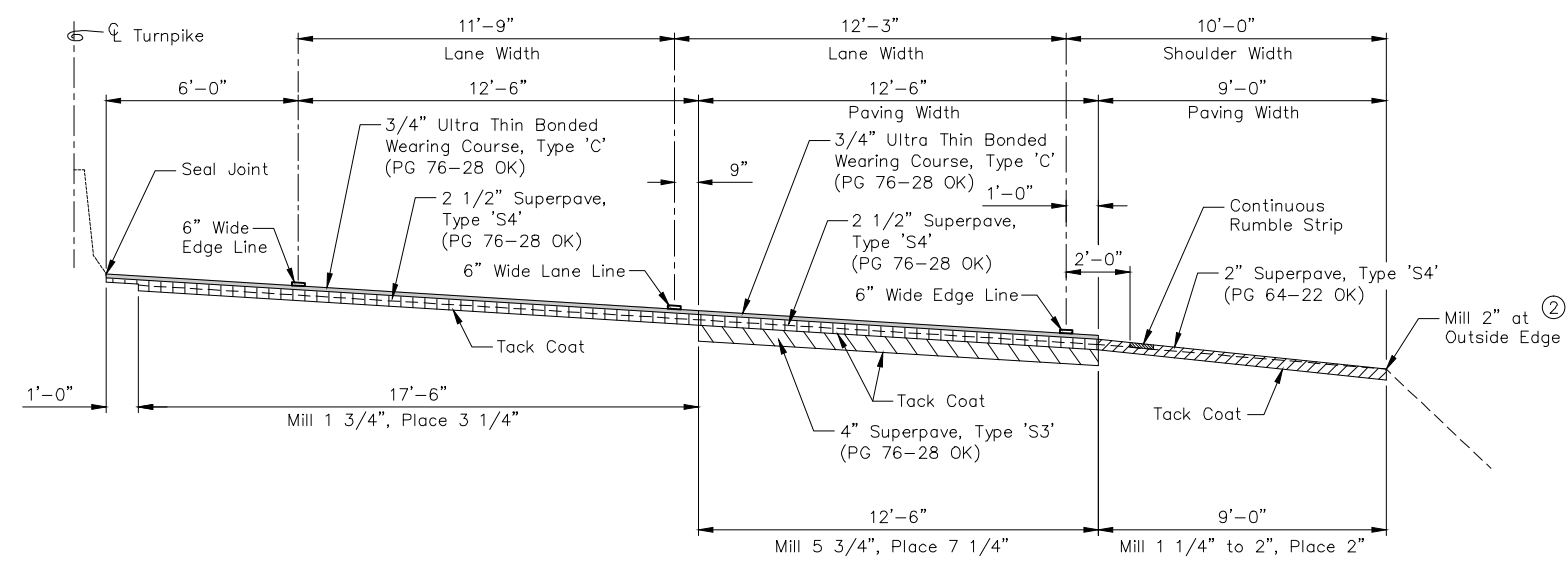
By _____ Date _____
Consulting Engineer

ACCEPTED BY
Oklahoma Turnpike Authority

By _____ Date _____
Chief Engineer

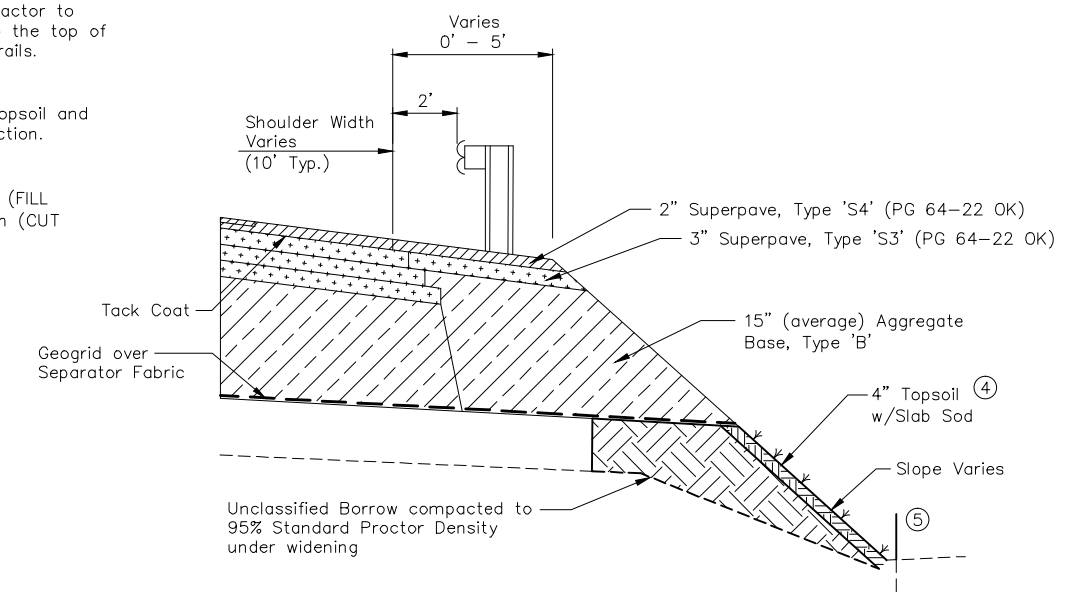
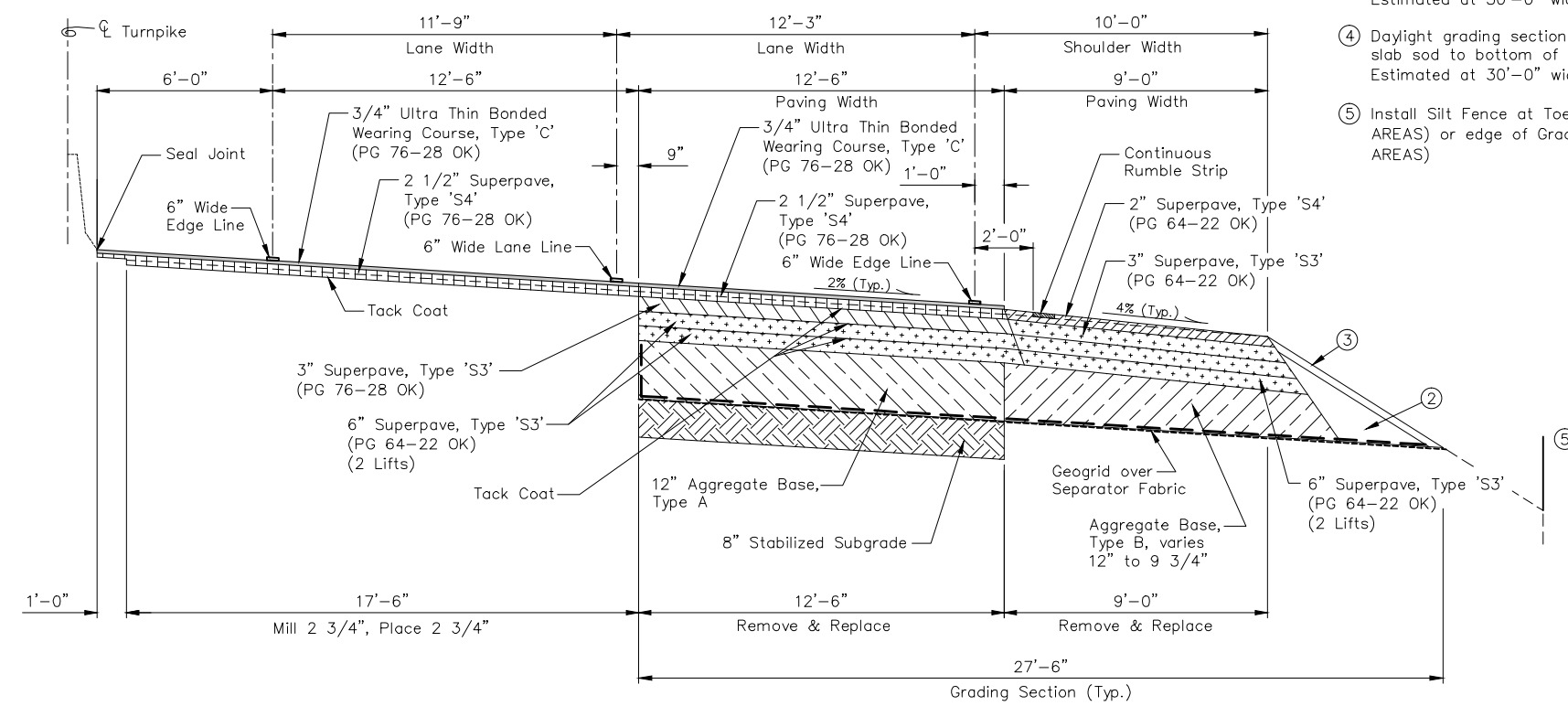


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NOTES:

- ① Topsoil and Solid Slab Sod to be used for shouldering up as required. Contractor to shoulder behind all guardrails to the top of the shoulder as directed by the Engineer. Estimated at 15'-0" wide.
- ② Shoulder up to finished grade using asphalt millings (compacted to achieve mechanical interlock) to fill all low areas / edge drop offs. Cost included in other items of work.
- ③ Topsoil and Slab Sod to be used for shouldering up as required. Contractor to shoulder full depth repair areas to the top of the shoulder, except behind guardrails. Estimated at 30'-0" wide.
- ④ Daylight grading section. Install topsoil and slab sod to bottom of grading section. Estimated at 30'-0" wide.
- ⑤ Install Silt Fence at Toe-of-Slope (FILL AREAS) or edge of Grading Section (CUT AREAS)

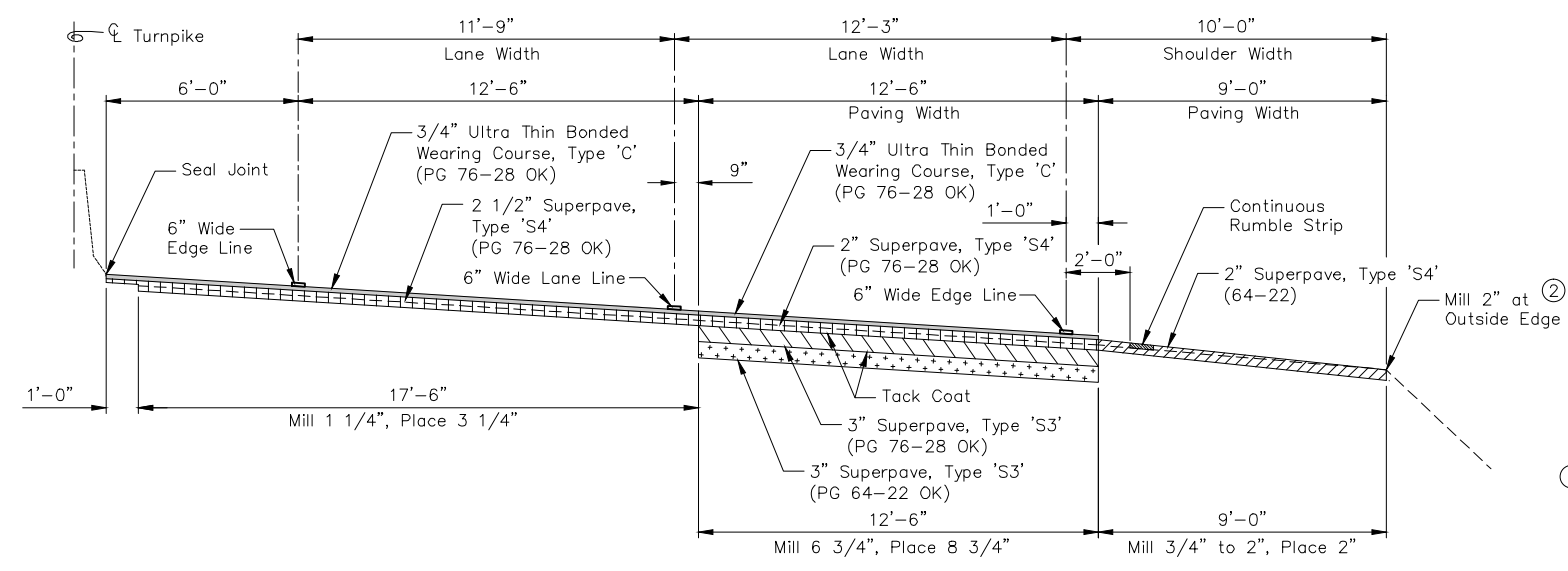


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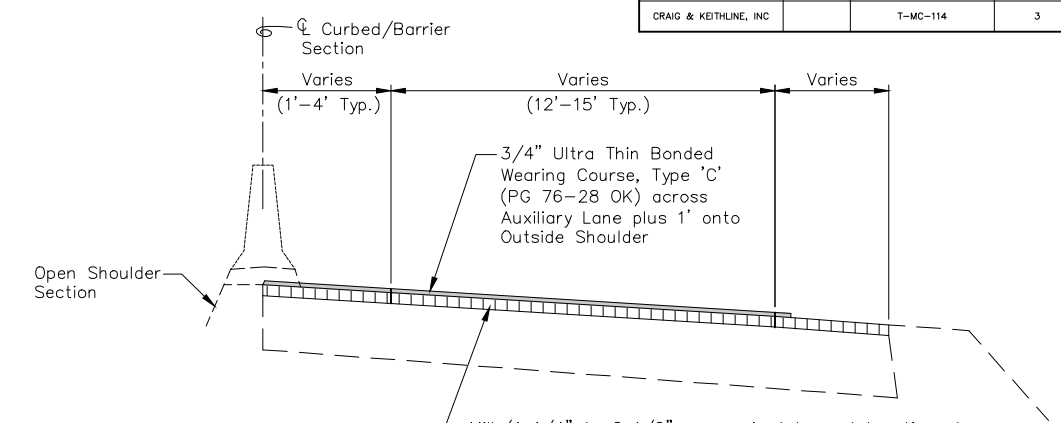
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NA		TYPICAL SECTIONS	
PROFILE SCALE HORIZONTAL NA			
VERTICAL NA			
CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 2 OF 33 SHEETS	

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TYPICAL SECTION 3 (EASTBOUND)
(MILL & OVERLAY)
N.T.S.

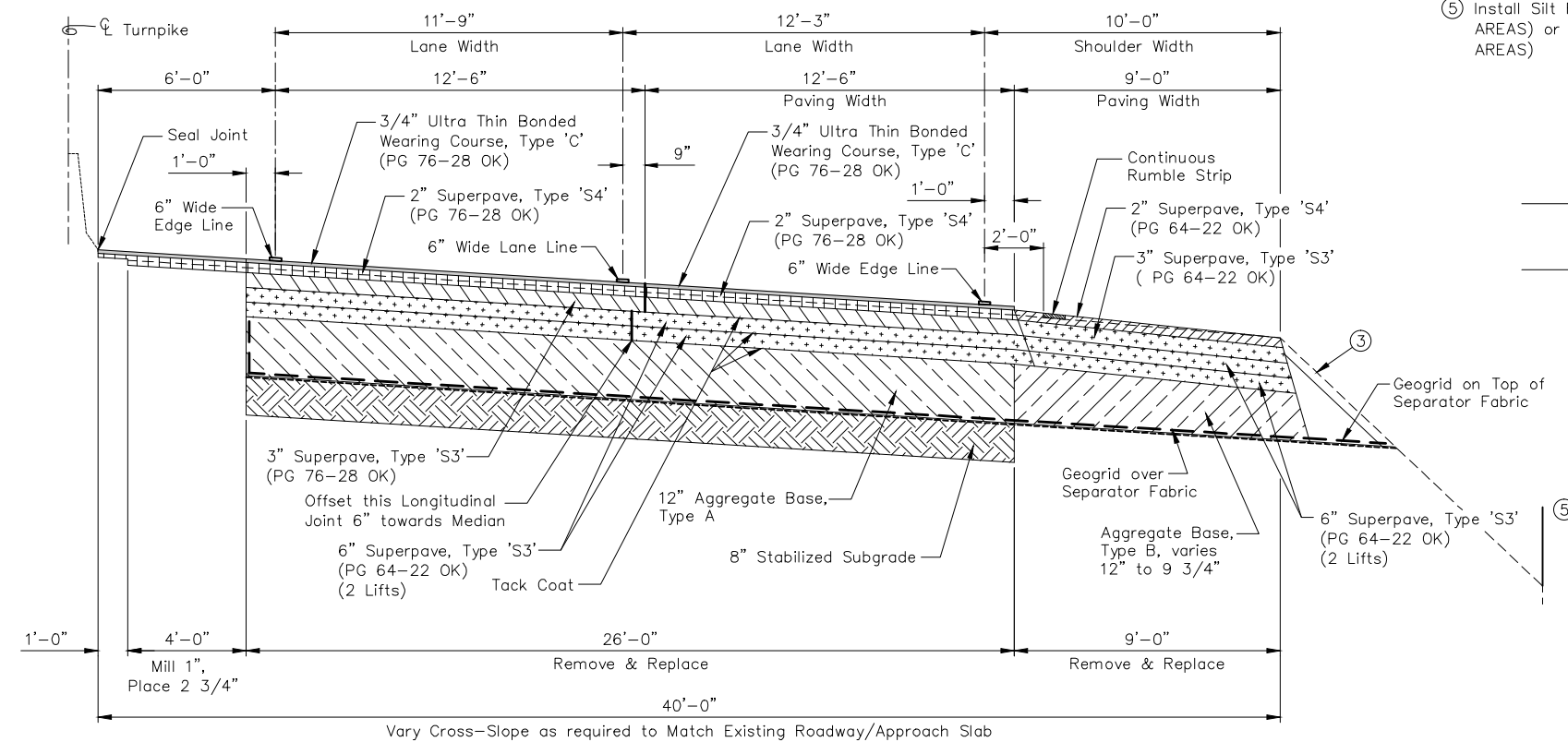
EASTBOUND 1680+10 - 1845+95 (16,585')



TYPICAL SECTION 5
(RAMP MILL & OVERLAY)
N.T.S.

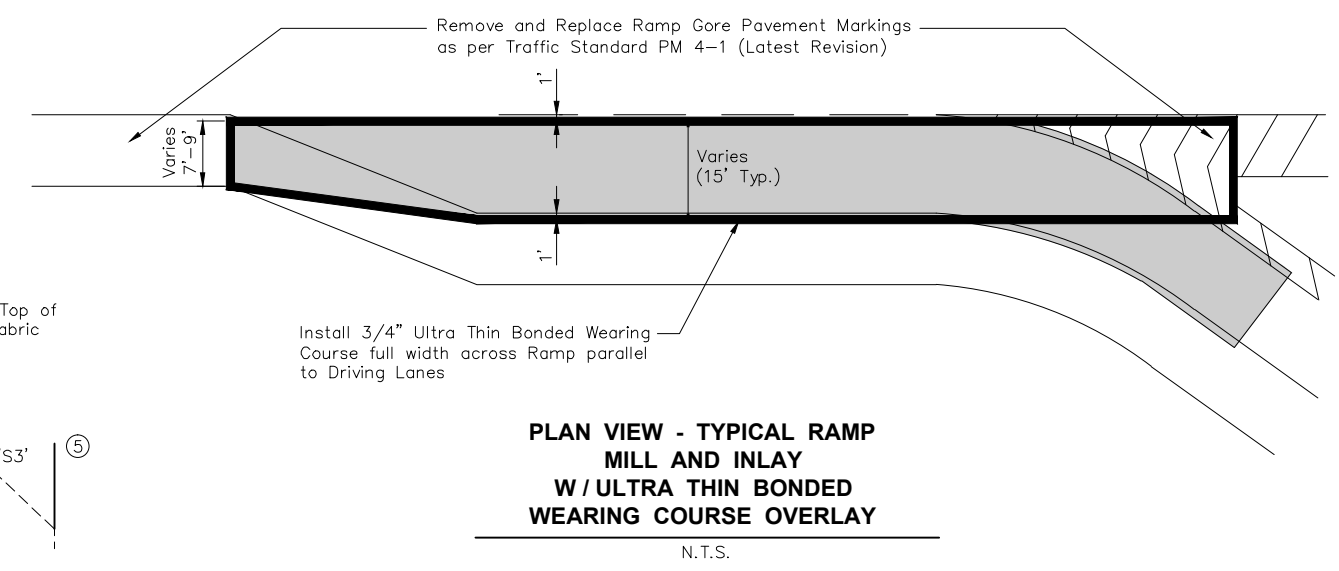
Mill (1 1/4" to 2 1/2" as required to match adjacent pavement at Ramp Terminals - 2 1/2" typical through main body of Ramp in Right-Hand Super-Elevated areas, vary mill depth as required to mill Ramp and both Shoulders at the same cross-slope (3% typical). In Left-Hand Super-Elevated areas mill 2 1/2" typical and maintain existing outside Shoulder cross-slope. Inlay 2 1/2" Superpave, Type S4 (PG 76-28 OK). Overlay 3/4" UTBWC across Auxiliary Lane portion of Ramp.

- NOTES:**
- Topsoil and Solid Slab Sod to be used for shouldering up as required. Contractor to shoulder behind all guardrails to the top of the shoulder as directed by the Engineer. Estimated at 15'-0" wide.
 - Shoulder up to finished grade using asphalt millings (compacted to achieve mechanical interlock) to fill all low areas / edge drop offs. Cost included in other items of work.
 - Topsoil and Slab Sod to be used for shouldering up as required. Contractor to shoulder full depth repair areas to the top of the shoulder, except behind guardrails. Estimated at 30'-0" wide.
 - Daylight grading section. Install topsoil and slab sod to bottom of grading section. Estimated at 30'-0" wide.
 - Install Silt Fence at Toe-of-Slope (FILL AREAS) or edge of Grading Section (CUT AREAS)



TYPICAL SECTION 4
(FULL DEPTH REPAIR - BOTH LANES)
N.T.S.

EASTBOUND 1664+00 - 1666+00 (200')
WESTBOUND 1664+20 - 1666+20 (200')



PLAN VIEW - TYPICAL RAMP
MILL AND INLAY
W/ ULTRA THIN BONDED
WEARING COURSE OVERLAY
N.T.S.

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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NA	TYPICAL SECTIONS		
PROFILE SCALE HORIZONTAL NA VERTICAL NA			
CRAIG & KEITHLINE, INC. Tulsa, Oklahoma			
DESIGNED DRAWN CHECKED	CKE CKD CKD	CONTRACT NO. DATE:	T-MC-114 SHEET 3 OF 33 SHEETS

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ROADWAY PART 'A'		PAY QUANTITIES		
ITEM	DESCRIPTION	UNITS	QUANTITY	
201(A)	CLEARING AND GRUBBING	L. SUM		1
202(A)	UNCLASSIFIED EXCAVATION (0-1)(U-1)	C.Y.		13,500
202(B)	MUCK EXCAVATION (U-2)	C.Y.		100
202(D)	UNCLASSIFIED BORROW (0-1)	C.Y.		1,000
205(A)	TYPE A SALVAGED TOPSOIL	L. SUM		1
209	MACHINE GRADING	L.F.		9,397.18
221(C)	TEMPORARY SILT FENCE (0-2)	L.F.		24,000
230(A)	SOLID SLAB SODDING (R-7)(R-8)	S.Y.		75,000
303(A)	AGGREGATE BASE TYPE A (0-3)	C.Y.		6,730
303(B)	AGGREGATE BASE TYPE B (0-3)	C.Y.		5,420
307(K)	STABILIZED SUBGRADE	S.Y.		20,200
325	SEPARATOR FABRIC	S.Y.		35,800
326(B)	GEOGRID REINFORCEMENT (TYPE 2)	S.Y.		35,800
404(G)	ULTRA-THIN BONDED WEARING COURSE, TYPE C (PG 76-28 OK)(U-14)	TON		9,600
411(B)	SUPERPAVE, TYPE S3 (PG 64-22 OK) (R-32)(U-13)	TON		18,500
411(B)	SUPERPAVE, TYPE S3 (PG 76-28 OK) (R-32)(U-13)	TON		17,000
411(C)	SUPERPAVE, TYPE S4 (PG 64-22 OK) (R-32)(U-13)	TON		8,600
411(C)	SUPERPAVE, TYPE S4 (PG 76-28 OK) (R-32)(U-13)	TON		29,300
411(H)	SUPERPAVE, TYPE S3 (PATCHING)(PG 76-28 OK) (U-13)	TON		500
412	COLD MILLING PAVEMENT (U-3)	S.Y.		255,800
413(A)	RUMBLE STRIP-METHOD HMA-CON	L.F.		61,300
509(B)	CLASS A CONCRETE	C.Y.		5
509(D)	CLASS C CONCRETE (U-6)	C.Y.		10
511(A)	REINFORCING STEEL	LB.		200
512(A)	PAINTING EXISTING STRUCTURES (U-4)	L. SUM		1
601(B)	TYPE I-A - PLAIN RIPRAP (24-INCH)	TON		150
601(C)	TYPE I-A - FILTER BLANKET (6-INCH)	TON		50
601(I)	FILTER FABRIC (RIPRAP)	S.Y.		150
619(B)	REMOVAL OF ASPHALT PAVEMENT (R-49)(R-50)	S.Y.		34,300
619(B)	REMOVAL OF GUARDRAIL (R-49)	L.F.		9,955
623(A)	BEAM GUARDRAIL W-BEAM SINGLE (0-7)	L.F.		9,422.10
623(F)	GUARDRAIL ANCHOR (0-7)(U-7)	EA.		21
623(G)	GUARDRAIL END TREATMENT (0-7)(U-8)	EA.		23
623(I)	GUARDRAIL BRIDGE CONNECTION (0-7)(U-10)	EA.		14
627(A)	CONCRETE LONGITUDINAL BARRIER, DESIGN 1	L.F.		180

TRAFFIC PART 'A'		PAY QUANTITIES		
ITEM	DESCRIPTION	UNITS	QUANTITY	
805(D)	REMOVE AND RESET SIGN (U-5)	EA.		3
850(A)	SHEET ALUMINUM SIGNS (C-56)(C-60)	S.F.		163.00
850(B)	EXTRUDED ALUMINUM PANEL SIGNS	S.F.		30
851(C)	SQUARE TUBE POST (U-15)	L.F.		130
853	DELINEATORS (U-9)	EA.		122
855(A)	TRAFFIC STRIPE (PLASTIC)(6" WIDE) (TS-20)	L.F.		172,000
855(A)	TRAFFIC STRIPE (PLASTIC)(8" WIDE)	L.F.		9,500
855(A)	TRAFFIC STRIPE (PLASTIC)(12" WIDE) (U-16)	L.F.		2,400
855(B)	TRAFFIC STRIPE (PLASTIC)(ARROWS)	EA.		2
855(B)	TRAFFIC STRIPE (PLASTIC)(WORDS)	EA.		11
857(A)	CONSTRUCTION TRAFFIC STRIPE(PAINT) (0-10)(TC-1)(TC-17)	L.F.		181,400
857(F)	PAVEMENT MARKING REMOVAL (U-17)	L.F.		27,500
877(A)	DELIVER PORTABLE LONGITUDINAL BARRIER (0-11)(TC-1)(TC-2)	L.F.		14,100
877(B)	RELOCATION OF PORT. LONGITUDINAL BARRIER (0-11)(TC-1)(TC-2)	L.F.		1,000
880(A)	(SP) ARROW DISPLAY (TYPE C)	SD.		480
880(E)	(PL) CONSTRUCTION ZONE SEQUENTIAL LIGHTING SYSTEM (U-11,12)	L.SUM		1
880(J)	(SP) CONSTRUCTION TRAFFIC CONTROL (TC-25)(TC-33)	L.SUM		1
880(K)	(SP) SURVEILLANCE OF TRAFFIC CONTROL (TC-39)	SD.		180
880(L)	(SP) TRAFFIC SURVEILLANCE, POLICE (TC-65)	HOUR		2,000
882(A)	(SP) CHANGEABLE MESSAGE SIGN	SD.		112
	MESSAGE SIGN	SD.		2,880
	OR	SD.		3,420
	1	SD.		180

CONSTRUCTION PART 'A'		PAY QUANTITIES		
ITEM	DESCRIPTION	UNITS	QUANTITY	
106	QUALITY CONTROL AND ACCEPTANCE	L. SUM		1
220	SWPPP DOCUMENTATION AND MANAGEMENT	L. SUM		1
641	MOBILIZATION	L. SUM		1
642	CONSTRUCTION STAKING	L. SUM		1

SITE RENTAL PART 'B'		PAY QUANTITIES		
ITEM	DESCRIPTION	UNITS	QUANTITY	
645(A)	PROJECT SITE RENTAL	\$/PSRD		\$10,000
430	PAVEMENT SMOOTHNESS (NON-BIDDABLE) (U-430)			

PAY ITEM NOTES

- (0-1) INCLUDES AN ESTIMATED 1,000 CY TO BE USED FOR UNDERCUTTING AND BACKFILLING AREAS OF UNSUITABLE MATERIAL AS DIRECTED BY THE ENGINEER.
- (0-2) ESTIMATED QUANTITY ONLY. TO BE USED AS DIRECTED BY THE ENGINEER. PRICE BID FOR THIS ITEM SHALL INCLUDE THE COST OF SEDIMENT REMOVAL WHEN THE DEVICE IS HALF-FULL OR AS DIRECTED BY THE ENGINEER.
- (0-3) TO BE MEASURED AND PAID BASED ON THE THEORETICAL CROSS-SECTION SHOWN ON THE TYPICAL SECTION MULTIPLIED BY THE ACTUAL LENGTH INSTALLED.
- (0-7) PRICE BID TO INCLUDE GUARDRAIL DELINEATORS PLACED AT 50' SPACING. GUARDRAIL DELINEATORS SHALL BE "PEXCO I-FLEX" DELINEATORS OR APPROVED EQUAL.
- (0-10) THIS ITEM IS TO BE USED AS APPROVED BY THE ENGINEER TO DIRECT TRAFFIC DURING THE VARIOUS STAGES OF CONSTRUCTION. CONTRACTOR MAY USE FLEX TABS AT HIS OPTION. CONTRACTOR SHALL USE FLEX-TABS FOR TEMPORARY MARKINGS AS SHOWN ON THE PLANS AND ON ALL FINISHED SURFACES. PAYMENT WILL BE BY THE LINEAR FOOT OF PAINT REGARDLESS OF THE ITEM USED.
- (0-11) PLAN QUANTITIES ARE BASED ON THE PLAN SEQUENCE OF CONSTRUCTION. FINAL PAYMENT WILL BE BASED ON THE ACTUAL AMOUNT USED IN ACCORDANCE WITH THE APPROVED TRAFFIC CONTROL PLAN.
- (R-7) PRICE BID TO INCLUDE COST OF 10-20-10 FERTILIZER, ESTIMATED AT 200 LBS. PER 1,000 SQ. YD.
- (R-8) PRICE BID TO INCLUDE COST OF WATERING, ESTIMATED AT 40 GALLONS PER SQ. YD.
- (R-32) ESTIMATED AT 112 LBS. PER SQ. YD. PER 1" THICK.
- (R-49) TO BECOME THE PROPERTY OF AND BE DISPOSED OF BY THE CONTRACTOR IN A MANNER APPROVED BY THE ENGINEER.
- (R-50) MATERIALS REMOVED SHALL NOT BE MEASURED FOR PAYMENT UNDER SECTION 202.06 UNCLASSIFIED EXCAVATION
- (TC-1) THE CONTRACTOR SHALL FURNISH AND INSTALL SUCH LIGHTS, SIGNS, BARRICADES, AND PROVIDE FLAGGERS NECESSARY FOR THE CONTROL, SAFETY, AND MAINTENANCE OF TRAFFIC WHEN INSTALLING, RELOCATING OR DELIVERING PRECAST CONCRETE MEDIAN BARRIER
- (TC-2) QUANTITY INCLUDES SUFFICIENT LENGTH OF PORTABLE LONGITUDINAL BARRIER TO PROVIDE FOR THE LONGEST SECTION SHOWN ON THE PLANS. THIS SAME BARRIER WILL BE USED ON OTHER DETOUR PHASES.
- (TC-17) INCLUDES AN ESTIMATED 118,000 L.F. (PAINT) (4" WIDE) WHITE 63,400 L.F. (PAINT)(4" WIDE) YELLOW STRIPE.
- (TC-25) CONSTRUCTION TRAFFIC CONTROL WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, AND APPLICABLE O.D.O.T. STANDARD DRAWINGS. PRICE BID FOR THIS ITEM SHALL BE PAYMENT IN FULL FOR THE INSTALLATION, MAINTENANCE AND SUBSEQUENT REMOVAL OF ALL NECESSARY CONSTRUCTION TRAFFIC CONTROL DEVICES AND PAVEMENT MARKINGS REQUIRED FOR COMPLETION OF THE PROJECT.

ALL SIGNS, BARRICADES, AND CHANNELIZING DEVICES WHICH ARE SHOWN WITH EITHER TYPE 'A' OR TYPE 'C' LIGHTS IN THE STANDARD DRAWINGS SHALL HAVE THE CORRESPONDING LIGHT ATTACHED DURING NON-DAYLIGHT HOURS.
- (TC-33) ALL CONSTRUCTION WORK ZONE SIGNS AND CHANNELIZING DEVICES SHALL HAVE FLUORESCENT SHEETING. THE FLUORESCENT SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956 (LATEST REVISION) FOR TYPE X REBOUNDABLE SHEETING.

THE MANUFACTURER SHALL FURNISH A TYPE 'D' CERTIFICATION IN ACCORDANCE WITH O.T.A. STANDARD SPECIFICATIONS (CURRENT EDITION) SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON MATERIAL SUBMITTED FOR APPROVAL.
- (TC-39) THE CONTRACTOR SHALL PROVIDE A PERSON, 24 HOURS A DAY, SEVEN DAYS A WEEK, AT THE CONSTRUCTION SITE TO MAINTAIN AND KEEP ALL TRAFFIC CONTROL DEVICES IN POSITION ANYTIME TRAFFIC IS DIRECTED AWAY FROM THE NORMAL TRAFFIC LANES OR ANYTIME THE ENGINEER DEEMS IT NECESSARY. THIS PERSON SHALL HOLD A CURRENT CERTIFICATION FROM THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION AS A TRAFFIC CONTROL TECHNICIAN OR TRAFFIC CONTROL SUPERVISOR.
- (TC-65) THE PRICE BID FOR THIS ITEM SHALL INCLUDE THE FOLLOWING:
 - ONE OFFICIALLY MARKED OKLAHOMA HIGHWAY PATROL CAR (WHEN PROJECT INVOLVES A STATE OR FEDERAL HIGHWAY) OR A LOCAL CITY OR COUNTY LAW ENFORCEMENT VEHICLE. PRICE BID FOR THIS ITEM SHALL BE PAID ON A PER UNIT PER HOUR BASIS.
 - ONE LAW ENFORCEMENT OFFICER WITH JURISDICTIONAL AUTHORITY TO WRITE AND ISSUE TRAFFIC CITATIONS. THE LAW ENFORCEMENT OFFICER SHALL BE INSURED, LICENSED AND BONDED, IF REQUIRED, BY THE CONTRACTOR. THIS OFFICER SHALL BE SPECIFICALLY APPROVED AND ASSIGNED TO THIS WORK ACTIVITY.
 - THE CONTRACTOR SHALL MAKE ALL THE NECESSARY ARRANGEMENTS WITH A LAW ENFORCEMENT AGENCY TO PROVIDE THE REQUIRED LAW ENFORCEMENT ON THIS PROJECT.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS ANTICIPATED WEEKLY SCHEDULE TO THE ENFORCEMENT AGENCY TWO WEEKS IN ADVANCE OF THE WORK. THE WORK SCHEDULE WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.
 - THE LAW ENFORCEMENT AGENCY WILL BE PAID FOR A MAXIMUM OF ONE (1) HOUR, PER WORK PERIOD, TO ALLOW FOR TRAVEL TO AND FROM THE OFFICER'S PERMANENT DUTY STATION AND THE WORK SITE. THIS WILL BE PAID ONE (1) TIME PER WORK PERIOD AS DEFINED BY THE CONTRACTOR IN AGREEMENT WITH THE ENGINEER.

PAY ITEM NOTES (continued)

- (C-56) ALL REGULATORY SIGNS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING. ALL WARNING SIGNS SHALL HAVE FLUORESCENT YELLOW SHEETING. THE FLUORESCENT YELLOW SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) REQUIREMENTS FOR TYPE VIII SHEETING. ALL GREEN AND BLUE SIGNS ON CONVENTIONAL HIGHWAYS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING. ALL PANEL AND OVERHEAD SIGNS SHALL HAVE TYPE III HIGH INTENSITY BACKGROUND WITH TYPE VIII LEGENDS AND BORDERS. THE TYPE III BACKGROUND AND THE TYPE VIII LEGENDS AND BORDERS SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION). THE MANUFACTURER SHALL FURNISH A TYPE 'A' CERTIFICATION IN ACCORDANCE WITH O.T.A. STANDARD SPECIFICATIONS, LATEST EDITION, SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON THE MATERIAL SUBMITTED FOR APPROVAL.
- (C-60) THE STATIONS AND LOCATIONS OF THE SIGN PLACEMENT, AS SHOWN ON THE PLAN SHEETS, ARE APPROXIMATE. EXACT STATIONS AND LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR SO THAT THE SIGN IS INSTALLED IN ACCORDANCE WITH O.T.A. STANDARDS AND THE MUTCD IN ORDER TO PROVIDE OPTIMUM VISIBILITY TO THE ONCOMING/APPROACHING MOTORIST. IF A PROPOSED LOCATION CONFLICTS WITH OTHER SIGNS, UTILITIES OR OTHER ROADWAY FEATURES, THE ENGINEER SHALL BE NOTIFIED.
- (TS-20) QUANTITY SHOWN INCLUDES 97,000 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 75,000 L.F. TRAFFIC STRIPE (PLASTIC)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (6") WIDE TRAFFIC STRIPE.
- (U-1) ESTIMATE IS BASED ON AVERAGE EXCAVATION DEPTH OF 12-INCHES IN FULL DEPTH REPAIR AREAS.
- (U-2) INCLUDES COST TO FLUSH CROSS DRAIN (WHERE SHOWN), REMOVE AND DISPOSE OF SEDIMENT IN AN APPROVED LOCATION.
- (U-3) MILLING DEPTHS SHALL BE ADJUSTED AS REQUIRED TO PENETRATE INTO UNDERLYING LAYER AND TO MAINTAIN VERTICAL CLEARANCES AT STRUCTURES. MILLINGS NOT USED FOR GUARDRAIL WIDENING OR SHOULDERING OPERATIONS BECOME THE PROPERTY OF THE CONTRACTOR.
- (U-4) INCLUDES COST TO CLEAN, PREP, AND PAINT 32,045' OF EXISTING CENTER MEDIAN BARRIERS, 1,000' OF EXISTING OUTSIDE SHOULDER BRIDGE PARAPETS, AND 180' OF NEW CONCRETE LONGITUDINAL BARRIERS. FEDERAL COLOR 36586 (OKLAHOMA STRAW). ALSO INCLUDES COST TO FURNISH AND INSTALL REFLECTORS ON TOP OF BARRIERS AS SHOWN IN PLAN DETAILS. ALSO INCLUDES COST TO GRIND TOP OF EXISTING PRE-CAST BARRIERS AS NEEDED PROVIDE FLUSH TRANSITION AT TOP OF BARRIER ADJACENT TO CAST-IN-PLACE BARRIER.
- (U-5) INCLUDES 2 EB MILEPOST SIGNS (S-1 AND S-2) AND REMOVAL ONLY OF 1 WB SIGN ASSEMBLY (S-6).
- (U-6) ESTIMATED QUANTITY TO BE USED AS DIRECTED BY THE ENGINEER
- (U-7) TRAILING END TURNDOWN UNITS.
- (U-8) "ET-31 PLUS" (OR APPROVED EQUAL).
- (U-9) DELINEATORS SHALL BE PLACED EVERY TENTH OF A MILE, 18" FROM EDGE OF PAVEMENT AND SHALL CONSIST OF "PEXCO FLEXI-GUIDE MODEL 456" (OR APPROVED EQUAL). INCREMENTS SHALL BE MEASURED IN THE EASTBOUND DIRECTION, WITH MILEAGE CORRECTIONS AT THE END OF EACH MILE. WESTBOUND DELINEATORS SHALL BE PLACED DIRECTLY ACROSS FROM EASTBOUND. ALSO INCLUDED IS THE COST TO REMOVE THE EXISTING DELINEATORS, WHICH SHALL BECOME THE PROPERTY OF THE CONTRACTOR.
- (U-10) INCLUDES 2 (STANDARD) AND 12 (MODIFIED). SEE SHEET 9 FOR DETAILS.
- (U-11) CONSTRUCT THE SEQUENTIAL LIGHTING SYSTEM USING SYNCHROGUIDE SEQUENTIAL WORKZONE TAPER WARNING LAMPS, EMPKO LED WIRELESS CHANNELIZING SEQUENTIAL DEVICES (LWCSD), OR APPROVED EQUAL. INSTALL AND MAINTAIN THE SYSTEM IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- (U-12) THIS PAY ITEM WILL HAVE TWO SYSTEMS WITH THE MAXIMUM OF 25 LIGHTS IN EACH SYSTEM. THESE WILL BE PLACED ON THE DRUMS IN THE TAPER SECTION.
- (U-13) INCLUDES COST TO FURNISH AND INSTALL TACK COAT BETWEEN ASPHALT LIFTS.
- (U-14) ESTIMATED AT 85 LBS. PER S.Y. PER 3/4" THICK.
- (U-15) 2 1/4", 12 GAUGE.
- (U-16) INCLUDES 24" STOP BAR NEAR SH-66 TO BE PAID AS DOUBLE THE MEASURED LENGTH.
- (U-17) FOR USE IN PCC PAVING EXCAPTION AREAS (MAINLINE AND BRISTOW SIDEGATE PLAZA). PRICE BID FOR PAVEMENT MARKING REMOVAL SHALL INCLUDE COST OF REMOVAL OF ARROWS, WORDS, AND SYMBOLS. THE PAVEMENT MARKING TO BE REMOVED SHALL BE CONSIDERED THERMOPLASTIC CONSISTING OF VARIOUS WIDTHS AND BID ACCORDINGLY.
- (U-430) PERFORM A PRELIMINARY PROFILOGRAPH ON THE FINAL LIFT OF S4 SUPERPAVE AND PERFORM SURFACE CORRECTIONS PRIOR TO THE PLACEMENT OF THE UTBWC AS REQUIRED AND APPROVED BY THE ENGINEER. AFTER THE UTBWC HAS BEEN PLACED, PERFORM A FINAL PROFILOGRAPH ACCORDING TO SECTION 430 OF THE STANDARD SPECIFICATIONS FOR TURNPIKE CONSTRUCTION EDITION 2010. NO SURFACE CORRECTIONS ON THE UTBWC WILL BE ALLOWED.



NO.		REVISION		BY		DATE	
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE							
PLAN SCALE		PAY QUANTITIES AND NOTES					
PROFILE SCALE							
HORIZONTAL							
VERTICAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma				SECTION	
DESIGNED	CKE	CONTRACT NO.		T-MC-114			
DRAWN	CKD	DATE:		SHEET 4 OF 33 SHEETS			
CHECKED							

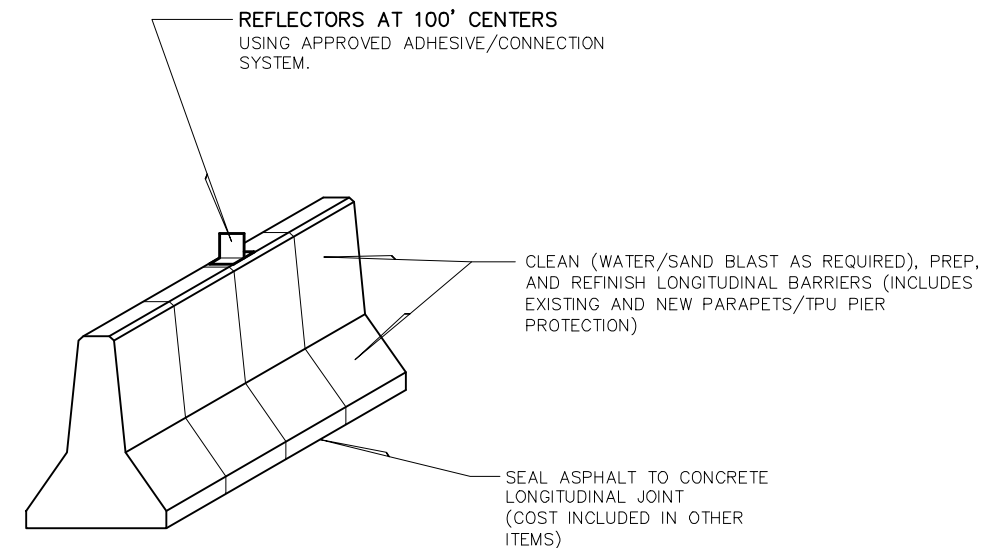
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GENERAL CONSTRUCTION NOTES

OKLAHOMA TURNPIKE AUTHORITY				
SECTION ENGINEER	SECTION	CONTRACT NUMBER	SHEET NO.	TOTAL SHEETS
CRAIG & KEITHLINE, INC		T-MC-114	5	33

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE OKLAHOMA TURNPIKE AUTHORITY STANDARD SPECIFICATIONS FOR TURNPIKE CONSTRUCTION, 2010 EDITION.
2. CONTRACTOR SHALL KEEP THE OPEN TRENCH DRAINED. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.
3. THE CONTRACTOR IS RESPONSIBLE FOR THE FOLLOWING:
 - A. PROPER NOTIFICATION OF ALL NECESSARY AGENCIES PRIOR TO CONSTRUCTION.
 - B. THE EXACT LOCATION OF ANY UTILITY LINES AND THEIR LOCATION NECESSITATED BY THE CONSTRUCTION.
4. CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN TRAVELED SURFACES OF TURNPIKE AND LOCAL STREETS CLEAR OF ROCK, DIRT AND DEBRIS AT ALL TIMES. THIS INCLUDES ROUTINE DAY-TO-DAY MAINTENANCE OF ANY COUNTY ROADS UTILIZED BY THE CONTRACTOR.
5. CONTRACTOR SHALL PROVIDE MAINTENANCE OF ROADWAY FOR THE LENGTH OF THE ENTIRE PROJECT IN ACCORDANCE WITH SECTION 104.07 OF THE SPECIFICATIONS. THIS INCLUDES PATCHING OF EXISTING DRIVING LANES AND SHOULDERS AS REQUIRED.
6. THE TOP LIFT OF SUPERPAVE ASPHALT IS CONSIDERED A "SURFACE LAYER" AND RECYCLED ASPHALT WILL NOT BE PERMITTED, BUT THE INSOLUBLE AGGREGATE REQUIREMENT WILL WAIVED DUE TO THE UTBWC SURFACE COARSE.
7. ALL WORK ON THIS PROJECT SHALL BE UNDERTAKEN SATURDAY THROUGH THURSDAY DURING THE HOURS OF 7:00 P.M. TO 6:00 A.M. EXCEPT AS NOTED HEREIN. NO WORK WILL BE PERMITTED DURING THE DAYTIME HOURS FROM 6:00 A.M. TO 7:00 P.M. EXCEPT AS NOTED HEREIN. THE FOLLOWING WORK ITEMS WILL BE PERMITTED DURING DAYTIME HOURS PROVIDED NO CONSTRUCTION HAUL VEHICLES ENTER OR EXIT THE MAINLINE ROADWAY DURING THE OPERATIONS:
 - REMOVAL, EXCAVATION, EMBANKMENT, GRADING, SURFACING, AND GUARDRAIL INSTALLATION IN THE FULL DEPTH REPAIR AREAS.
 - SIGN INSTALLATION.
 - DRAINAGE STRUCTURE REPAIRS OUTSIDE THE CLEAR ZONE (30- FEET FROM THE EDGE OF THE CLOSEST DRIVING LANE).
8. CAPACITY REQUIREMENTS TO BE OBSERVED DURING THE MAINLINE SUPERPAVE MILL & INLAY OPERATIONS ARE AS FOLLOWS:
 - DURING THE SUPERPAVE MILL & INLAY OPERATIONS ONLY, OTA WILL ALLOW THE LANE CLOSURE TO REMAIN IN PLACE, WITH NO LANE RENTAL FEE, UNTIL THE TOP LIFT OF S4 HAS BEEN PLACED. THE MAXIMUM ALLOWABLE EDGE DROP OFF REMAINS AT 3", SO THE INLAY WILL HAVE TO BE PLACED EACH WORKING PERIOD TO ACCOMMODATE NO MORE THAN THAT AMOUNT IN CASE A VEHICLE ENTERS THE WORKZONE.
 - THIS LANE CLOSURE LENGTH WILL BE LIMITED TO A MAXIMUM OF 3 MILES, INCLUDING THE TAPER LENGTHS. THE LANE CLOSURE SHALL BE LIMITED TO ONLY THE PORTION OF THAT 3 MILES THAT HAS AN EDGE DROP OFF.
 - ONCE A 3 MILE SECTION HAS RECEIVED ALL OF THE SUPERPAVE, THE LANE CLOSURE WILL BE LIFTED, LEAVING A 3/4" EDGE LIP (WB) AND A 1 1/4" EDGE LIP (EB SEGMENT 2) UNTIL THE ADJACENT LANE IS PAVED.
 - ALL WORKERS, EQUIPMENT, MATERIALS, ETC. MUST BE OFF THE ROADWAY OUTSIDE OF THE ALLOWABLE WORKING HOURS, 7:00 P.M. TO 6:00 A.M., OR THE LANE RENTAL FEE WILL BE ASSESSED.
 - EXCLUDING THE FULL DEPTH REPAIR AREAS, ALL DRIVING LANES MUST BE OPENED TO FULL CAPACITY FRIDAY MORNING BY 6:00 A.M. AND REMAIN SO UNTIL THE SATURDAY EVENING WORK SHIFT BEGINS AT 7:00 P.M., OR THE LANE RENTAL FEE WILL BE ASSESSED.
 - THE UTBWC AND PAVEMENT MARKING OPERATIONS CAN UTILIZE A MAXIMUM LANE CLOSURE LENGTH OF 6 1/4 MILES, INCLUDING THE TAPER LENGTHS.
9. CAPACITY REQUIREMENTS TO BE OBSERVED DURING THE BRISTOW INTERCHANGE RAMP REPAIRS ARE AS FOLLOWS:
 - THE CONTRACTOR WILL BE ALLOWED TO CLOSE THE BRISTOW INTERCHANGE RAMP ONE-AT-A-TIME DURING THE HOURS OF 8:00 P.M. TO 6:00 A.M. (MONDAY, TUESDAY, AND THURSDAY NIGHTS ONLY) TO PERFORM THE MILL, PATCH, AND INLAY.
 - ONLY THAT PORTION OF ANY RAMP THAT CAN BE COMPLETED IN A WORK PERIOD SHALL BE ATTEMPTED. EACH RAMP MUST BE RE-OPENED TO FULL CAPACITY BY 6:00 A.M. OR THE LANE RENTAL FEE WILL BE ASSESSED.
 - THE RAMP TERMINALS AT STATE HIGHWAY 66 MUST REMAIN OPEN TO BOTH DIRECTIONS AT ALL TIMES (FLAGMEN MAY BE UTILIZED TO FACILITATE THIS OPERATION).
 - THE CONTRACTOR SHALL UTILIZE CHANGEABLE MESSAGE BOARDS TO PROVIDE A 2-WEEK NOTICE TO THE TRAVELING PUBLIC CONCERNING EACH RAMP CLOSURE.
10. CAPACITY REQUIREMENTS TO BE OBSERVED DURING ALL OTHER OPERATIONS ARE AS FOLLOWS:
 - THE CONTRACTOR SHALL HAVE NO MORE THAN 3.0 MILES OF ROADWAY WORK ZONE UNDER CONSTRUCTION FOR EACH TRAVEL DIRECTION AT ONE TIME.
 - THE CONTRACTOR MAY HAVE MULTIPLE WORK ZONES IN THE SAME TRAVEL DIRECTION AS LONG AS A 2.0 MILE MINIMUM SEPARATION DISTANCE, INCLUDING TAPER LENGTHS, IS OBSERVED.
11. AT ALL EXISTING CROSSOVER LOCATIONS, RELOCATE THE PRECAST MEDIAN BARRIERS AS REQUIRED AND RE-INSTALL THEM ON TOP OF THE FINAL SURFACE COARSE. RELOCATION WILL BE PAID AS "RELOCATION OF PORT. LONGITUDINAL BARRIER". SHAPING OF TOPS TO ELIMINATE SNAGGING HAZARD IS INCIDENTAL TO THE PAY ITEM "PAINTING EXISTING STRUCTURES".

- (G-48) CONTRACTOR SHALL NOT UTILIZE EMERGENCY VEHICLE TURNAROUNDS ON THE TURNPIKE
- (G-49) TRUCK MOUNTED ATTENUATORS WILL BE USED TO PROTECT AREAS EXPOSED TO TRAFFIC WHEN SAND FILLED IMPACT ATTENUATORS OR GUARDRAIL ARE REMOVED UNLESS BARRIER IS ALREADY IN PLACE. COST TO BE INCLUDED IN OTHER ITEMS OF WORK. THE CONTRACTOR SHALL NOT LEAVE ANY EXPOSED ENDS OF STRUCTURES. FAILURE TO ADEQUATELY PROTECT THESE TRAFFIC HAZARDS WILL RESULT IN A SHUTDOWN NOTICE BEING ISSUED.
- (G-51) THE CONTRACTOR SHALL SUBMIT A FINAL TRAFFIC CONTROL AND CONSTRUCTION PHASING PLAN TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO BEGINNING CONSTRUCTION DESCRIBING ALL WORK PHASES FOR THE CONSTRUCTION OF THE BRIDGES AND ROADWAYS AFFECTED.
- (G-52) LANE LINES, AT A MINIMUM, SHALL BE ESTABLISHED AT ALL TIMES DURING CONSTRUCTION.
- (G-53) CONTRACTOR SHALL DOCUMENT PRE-CONSTRUCTION SITE CONDITIONS BY MEANS OF PHOTOGRAPHS AND VIDEO WITH A DESIGNATED OTA REPRESENTATIVE, BEFORE THE START OF CONSTRUCTION. COST SHALL BE CONSIDERED AS INCIDENTAL AND NO SEPARATE PAYMENT SHALL BE MADE.
- (G-54) CONTRACTOR SHALL PERFORM ALL CONSTRUCTION STAKING REQUIRED FOR THE PROJECT. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.
- (G-55) CONTRACTOR SHALL PAY ALL TURNPIKE FEES. TOLL COST SHALL BE INCLUDED IN OTHER ITEMS OF WORK.
- (G-56) ALL WORK AND/OR MATERIALS NOT CLASSIFIED AS A "CONTRACT PAY ITEM" SHALL BE CONSIDERED INCIDENTAL AND THE COST THEREOF SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEMS WHICH ARE CLASSIFIED FOR PAYMENT.
- (G-57) CONTRACTOR SHALL DETERMINE FINAL VERTICAL CLEARANCES AT ALL TPU BRIDGES WITHIN THE LIMITS OF THE PROJECT, WITH THE ON-SITE REPRESENTATIVE, AND POST SAME AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.
- (G-59) GEOTECHNICAL SUBSURFACE INVESTIGATION REPORTS ARE AVAILABLE FOR REVIEW AT THE OFFICE OF CRAIG & KEITHLINE, INC. AND THE OFFICE OF THE OKLAHOMA TURNPIKE AUTHORITY IN OKLAHOMA CITY. THE REPORTS ARE FOR INFORMATIONAL PURPOSES ONLY. OTA CANNOT GUARANTEE THE ACCURACY OF THE SOIL CONDITIONS.
- (G-60) THE PRIME CONTRACTOR SHALL BE REPRESENTED ON SITE AT ALL TIMES. HE MUST BE PRESENT WHEN WORK IS BEING PERFORMED BY HIS SUBCONTRACTORS.
- (G-61) ALL FIELD MEASUREMENTS SHALL PREVAIL ON INSTALLATION AND REMOVAL.
- (G-62) CONTRACTOR SHALL BE ALLOWED TO USE OTA RIGHT-OF-WAY AT APPROVED LOCATIONS FOR STORAGE. SITE MUST BE RESTORED TO ORIGINAL CONDITION IMMEDIATELY FOLLOWING CONSTRUCTION.
- (G-63) IF THE CONTRACTOR ELECTS TO OPEN A GATE IN THE RIGHT-OF-WAY FENCE, THE GATE SHALL EITHER BE LOCKED OR MANNED AT ALL TIMES. IF THE GATE IS FOUND UNLOCKED OR UNMANNED THREE TIMES, THE GATE WILL BE PERMANENTLY LOCKED.
- (G-64) OTA RETAINS THE RIGHT TO KEEP ANY ROADS FROM THE TURNPIKE TO THE COUNTY ROADS CONSTRUCTED BY THE CONTRACTOR. ALL ACCESS ROADS SHALL BE APPROVED BY THE OTA PRIOR TO CONSTRUCTION.
- (G-65) THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY RIGHT-OF-WAY FENCE AS REQUIRED. WHEN THE PORTION OF THE PROJECT THAT REQUIRED THIS FENCE IS COMPLETED, THE TEMPORARY FENCE SHALL BE REMOVED, AND PERMANENT RIGHT-OF-WAY FENCING SHALL BE RESTORED OR INSTALLED IN A MANNER APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.
- (G-66) ALL COLD MILLED SURFACES AND TACK COAT APPLICATIONS SHALL BE OVERLAID WITH HOT MIX ASPHALT WITHIN THE SAME WORKING PERIOD, SUCH THAT THE MAXIMUM ELEVATION DIFFERENTIAL AT THE CLOSE OF DAILY OPERATIONS IS NO MORE THAN 2 INCHES.
- (G-68) ALL WORKERS, EQUIPMENT, MATERIALS, ETC MUST BE OFF THE ROADWAY AND OUTSIDE OF THE CLEAR ZONE DURING NON-ALLOWABLE WORKING HOURS. FAILURE TO COMPLY WILL RESULT IN THE APPLICATION OF A LANE RENTAL PENALTY AS OUTLINED IN SP 645.05.
- (G-71) ALL DIMENSIONS OF THE EXISTING BRIDGE AND ROADWAYS SHOWN ON THE PLANS ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, BOTH SURVEYED AND AS-BUILT, NECESSARY TO CONNECT THE NEW MATERIAL AND SHALL BE SOLELY RESPONSIBLE FOR THE ACCURACY THEREOF. REFER TO THE AS-BUILT PLANS FOR DIMENSIONS AND DETAILS NOT SHOWN IN THE PLANS.



MEDIAN BARRIER DETAIL

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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NA	GENERAL CONSTRUCTION NOTES & MEDIAN BARRIER DETAIL		
PROFILE SCALE HORIZONTAL NA VERTICAL NA			
		SECTION	
DESIGNED DRAWN CHECKED	CKE CKD	CONTRACT NO. T-MC-114	
		DATE:	SHEET 5 OF 33 SHEETS

T-MC-114 SUMMARY OF SURFACING													
Location (Station to Station)	303(A)	303(B)	307(K)	325	326(B)	404(G)	411(B)	411(B)	411(C)	411(C)	412	413(A)	619(B)
	Aggregate Base Type 'A'	Aggregate Base Type 'B'	Stabilized Subgrade	Separator Fabric	Geogrid Type 2	UTBWC (PG 76-28 OK)	Superpave Type S3 (PG 64-22 OK)	Superpave Type S3 (PG 76-28 OK)	Superpave Type S4 (PG 64-22 OK)	Superpave Type S4 (PG 76-28 OK)	Cold Milling	Rumble Strip AC-CON	Rem. of Asphalt Pavement
	C.Y.	C.Y.	S.Y.	S.Y.	S.Y.	TON	TON	TON	TON	TON	S.Y.	L.F.	S.Y.
SEGMENT 1, PHASE 1 (1527+20 - 1666+20)	6,530	5,350	19,600	35,000	35,000	0	14,300	3,400	1,900	2,200	0	0	33,500
SEGMENT 1, PHASE 2 (1525+50 - 1666+20)	200	70	600	800	800	0	300	100	50	6,900	53,600	0	800
SEGMENT 1, PHASE 3 (1525+50 - 1664+20)	0	0	0	0	0	0	0	4,400	1,650	2,700	33,200	0	0
SEGMENT 2, PHASE 1 (1680+10 - 1845+95)	0	0	0	0	0	0	0	0	0	8,200	65,000	0	0
SEGMENT 2, PHASE 2 (1680+10 - 1845+95)	0	0	0	0	0	0	3,900	9,100	3,800	5,800	79,300	0	0
SEGMENT 1 & 2, FINAL PHASE (1527+20 - 1845+95)	0	0	0	0	0	8,800	0	0	0	0	0	61,300	0
NON FULL DEPTH GUARDRAIL WIDENING	0	0	0	0	0	0	0	0	1,200	0	0	0	0
RAMP A (EB EXIT)	0	0	0	0	0	200	0	0	0	350	2,400	0	0
RAMP B (EB ENTRANCE)	0	0	0	0	0	200	0	0	0	800	5,700	0	0
RAMP C (WB EXIT)	0	0	0	0	0	200	0	0	0	1,100	7,700	0	0
RAMP D (WB ENTRANCE)	0	0	0	0	0	200	0	0	0	850	6,000	0	0
MISCELLANEOUS	0	0	0	0	0	0	0	0	0	400	2,900	0	0
TOTAL	6,730	5,420	20,200	35,800	35,800	9,600	18,500	17,000	8,600	29,300	255,800	61,300	34,300

T-MC-114 SUMMARY OF GUARDRAIL									
Number	Approximate Station Begin Rail	209	411(B)	619(B)	623(A)	623(F)	623(G)	623(I)	627(A)
		Machine* Grading	Superpave** Type S4 (PG 64-22 OK)	Rem. of Guardrail	Guardrail W-Beam Single	Guardrail Anchor	Guardrail End Treatment	Guardrail Bridge Connection	Long. Barrier Design 1
		L.F.	TON	L.F.	L.F.	EA.	EA.	EA.	L.F.
Eastbound									
G-1	1558+10 EB	0	0	385	312.50	1	1	0	0
G-2	1596+50 EB	0	0	360	287.50	1	1	0	0
G-3	1614+90 EB	0	0	455	387.50	1	1	0	0
G-4	1624+00 EB	0	0	355	221.88	0	1	1	90
G-5	1635+20 EB	0	0	685	612.50	1	1	0	0
G-6	1658+50 EB	0	0	740	675.00	0	1	1	0
G-7	1667+50 EB	100	10	100	62.50	0	0	2	0
G-8	1670+50 EB	161.25	20	160	112.50	1	0	1	0
G-9	1677+30 EB	190.63	30	200	75.00	0	1	1	0
G-10	1680+00 EB	161.25	20	80	112.50	1	0	1	0
G-11	1721+50 EB	489.38	60	430	362.50	1	1	0	0
G-12	1732+70 EB	376.88	50	325	250.00	1	1	0	0
G-13	1767+00 EB	351.88	50	250	225.00	1	1	0	0
G-14	1784+80 EB	389.38	50	230	262.50	1	1	0	0
G-15	1805+50 EB	814.38	100	760	687.50	1	1	0	0
G-16	DO NOT DISTURB	-	-	-	-	-	-	-	-
EB Subtotal		3,035.03	390	5,515	4,646.88	11	12	7	90
Westbound									
G-17	DO NOT DISTURB	-	-	-	-	-	-	-	-
G-18	1816+10 WB	764.38	100	710	637.50	1	1	0	0
G-19	1790+00 WB	389.38	50	200	262.50	1	1	0	0
G-20	1772+60 WB	439.38	60	380	312.50	1	1	0	0
G-21	1739+30 WB	401.88	50	350	275.00	1	1	0	0
G-22	1728+80 WB	401.88	50	350	275.00	1	1	0	0
G-23	1681+70 WB	203.13	30	90	87.50	0	1	1	0
G-24	1678+70 WB	790.84	100	430	753.34	0	0	2	0
G-25	COMBINE W/G-24	0	0	200	0	0	0	0	0
G-26	1669+20 WB	100	10	100	62.50	0	0	2	0
G-27	1666+40 WB	698.75	90	180	650.00	1	0	1	0
G-28	1630+00 WB	477.51	60	360	221.88	0	1	1	90
G-29	1622+00 WB	389.38	50	330	262.50	1	1	0	0
G-30	1603+30 WB	489.38	60	430	362.50	1	1	0	0
G-31	1664+70 WB	389.38	50	330	262.50	1	1	0	0
WB Subtotal		5,935.27	760	4,440	4,425.22	9	10	7	90
EB Exit Ramp									
G-32	1676+80	426.88	50	0	350	1	1	0	0
TOTAL		9,397.18	1,200	9,955	9,422.10	21	23	14	180

* MACHINE GRADING IN FULL DEPTH REPAIR AREAS IS INCIDENTAL TO REPAIR GRADING.
 ** ASPHALT IN FULL DEPTH REPAIR AREAS IS INCLUDED IN REPAIR QUANTITIES.

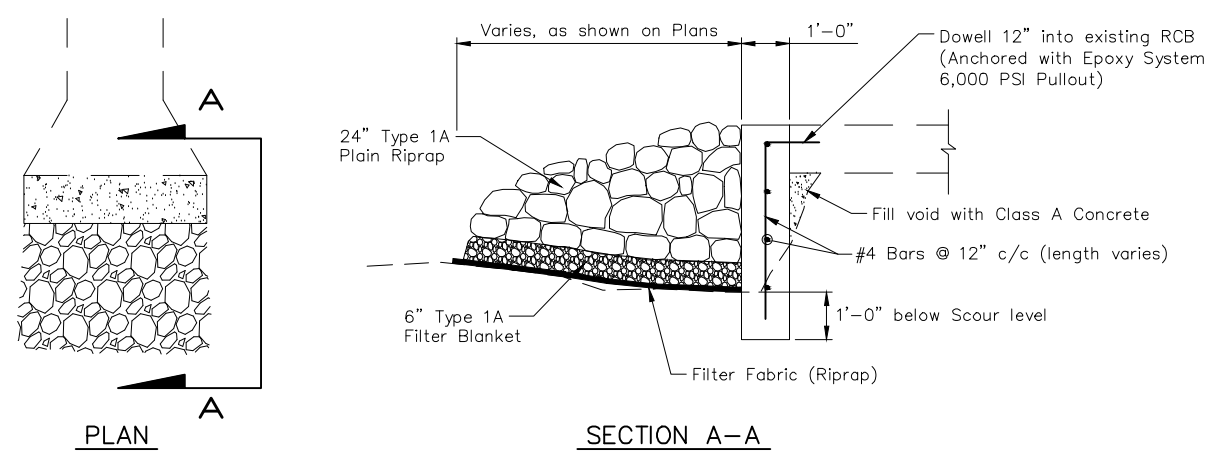
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OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NA	SUMMARIES OF GUARDRAIL, REPAIR AND SURFACING		
PROFILE SCALE HORIZONTAL NA VERTICAL NA			
CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		SECTION	
DESIGNED DRAWN CHECKED	CKE CKD	CONTRACT NO. T-MC-114	SHEET 6 OF 33 SHEETS

T-MC-113 SIGN SUMMARY								
Sign Number	Approx. Station	Direction	Designation	805(D)	850(A)	850(B)	851(C)	Description
				Remove & Reset Sign	Sheet Aluminum	Extruded Aluminum	Square Tube Post	
				EA.	S.F.	S.F.	LF.	
S-1	1578+50	EASTBOUND		1				"MILEPOST 195"
S-2	1630+90	EASTBOUND		1				"MILEPOST 196"
S-3	1659+40	EASTBOUND	W8-13F		16.00			REPLACE EXISTING SIGN ON EXISTING POST
S-4	1676+70	EASTBOUND	W13-2F (30)		20.00			REPLACE EXISTING SIGN ON EXISTING POST
S-5	1677+20	EASTBOUND	E13-1 (30)			15.00		7'6" x 2'0" - ADD TO EXISTING SIGN
S-6	1841+90	WESTBOUND		1				REMOVAL OF EXISTING "ROAD ICE/LAND GRAND" SIGN ONLY - NO REPLACEMENT
S-7	1691+50	WESTBOUND	W8-13F		16.00			REPLACE EXISTING SIGN ON EXISTING POST
S-8	1687+10	WESTBOUND	W13-2F (30)		20.00			REPLACE EXISTING SIGN ON EXISTING POST
S-9	1686+20	WESTBOUND	E13-1 (30)			15.00		7'6" x 2'0" - ADD TO EXISTING SIGN
S-10	1536+20	WESTBOUND	W8-13F		16.00			REPLACE EXISTING SIGN ON EXISTING POST
		EB EXIST RAMP A	W1-8E		45.00		78.00	NEW ASSEMBLIES - 6 EACH
		WB EXIT RAMP C	W1-8E		30.00		52.00	NEW ASSEMBLIES - 4 EACH
TOTALS				3.0	163.00	30.00	130.00	

T-MC-114 SUMMARY OF DRAINAGE STRUCTURES							
Number	Approximate Station	202(B)	509(B)	511(A)	601(B)	601(C)	601(I)
		Muck Excavation	Class 'A' Concrete	Reinforcing Steel	Type 1-A Plain Riprap	Type 1-A Filter Blanket	Filter Fabric (Riprap)
		C.Y.	C.Y.	LB.	TON	TON	S.Y.
CD-6	1585+50 EB	40	-	-	-	-	-
CD-7	1598+50 EB	-	2.78	120	130	30	100
CD-9	1625+50 EB	40	-	-	-	-	-
MISCELLANEOUS	AS DIRECTED	20	2.22	80	20	20	50
TOTAL		100	5	200	150	50	150



Typical Scour Repair
NTS

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OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NA	SUMMARIES OF DRAINAGE STRUCTURES AND SIGNING		
PROFILE SCALE HORIZONTAL NA VERTICAL NA	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		SECTION
DESIGNED DRAWN CHECKED	CKE CKD	CONTRACT NO. DATE:	T-MC-114 SHEET 7 OF 33 SHEETS

STORM WATER MANAGEMENT PLAN

OKLAHOMA TURNPIKE AUTHORITY				
SECTION ENGINEER	SECTION	CONTRACT NUMBER	SHEET NO.	TOTAL SHEETS
CRAIG & KEITHLINE, INC.		T-MC-114	8	33

SITE DESCRIPTION

PROJECT LIMITS: SECTIONS 24, 25, 26 T-16-N R-8-E
AND SECTIONS 9, 10, 15, 16, 19, 20, 21 T-16-N R-9-E

PROJECT DESCRIPTION: PAVEMENT REHABILITATION OF TURNER TURNPIKE (I-44)
MILEPOST 194.0 TO MILE 200.0. TOTAL LENGTH 6 MILES.

SUGGESTED SEQUENCE OF EROSION CONTROL ACTIVITIES: IN FULL DEPTH REPAIR AND GUARDRAIL WIDENING AREAS:

- 1) INSTALL AND MAINTAIN SILT FENCE ALONG TOE-OF SLOPE.
- 2) CONSTRUCT GRADING/SURFACING AND SHOULDER UP.
- 3) INSTALL SLAB SOD IN DISTURBED AREAS AS NEEDED.

SOIL TYPE: CLAY AND GRAVEL (GUARDRAIL WIDENING AREAS)

AREA TO BE DISTURBED: 1.27 ACRES

OFFSITE AREA TO BE DISTURBED: _____
 (FOR CONTRACTOR USE)

MAXIMUM ACRES TO BE DISTURBED AT ANY ONE TIME: _____
 (FOR CONTRACTOR USE)

LATITUDE & LONGITUDE OF CENTER OF PROJECT: 35° 51' N, 96° 24' W

NAME OF RECEIVING WATERS: CATFISH CREEK, SAND CREEK, ROCK CREEK

SENSITIVE WATERS OR WATERSHEDS: YES NO

303(d) IMPAIRED WATERS: YES NO

NOTE:
 THIS SHEET SHOULD BE USED IN CONJUNCTION WITH A DRAINAGE MAP THAT ILLUSTRATES THE DRAINAGE CHARACTERISTICS AND RECEIVING WATERS FOR THIS PROJECT. THIS SHEET SHOULD ALSO BE USED WITH THE EROSION CONTROL SUMMARIES, PAY ITEMS, & NOTES.

EROSION AND SEDIMENT CONTROLS

SOIL STABILIZATION PRACTICES:

- TEMPORARY SEEDING
- PERMANENT SODDING, SPRIGGING OR SEEDING
- VEGETATIVE MULCHING
- SOIL RETENTION BLANKET
- PRESERVATION OF EXISTING VEGETATION

NOTE: TEMPORARY EROSION CONTROL METHODS MUST BE USED ON ALL DISTURBED AREAS WHERE CONSTRUCTION ACTIVITIES HAVE CEASED FOR OVER 14 DAYS. METHODS USED WILL BE AS SHOWN ON PLANS, OR AS DIRECTED BY THE ENGINEER.

STRUCTURAL PRACTICES:

- STABILIZED CONSTRUCTION EXIT
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKES
- TEMPORARY BALE BARRIERS
- TEMPORARY FIBER LOG
- DIVERSION, INTERCEPTOR OR PERIMETER DIKES
- DIVERSION, INTERCEPTOR OR PERIMETER SWALES
- ROCK FILTER DAMS
- TEMPORARY SLOPE DRAIN
- PAVED DITCH W/ DITCH LINER PROTECTION
- TEMPORARY DIVERSION CHANNELS
- TEMPORARY SEDIMENT BASINS
- TEMPORARY SEDIMENT TRAPS
- TEMPORARY SEDIMENT FILTERS
- TEMPORARY SEDIMENT REMOVAL
- RIP RAP
- INLET SEDIMENT FILTER
- TEMPORARY BRUSH SEDIMENT BARRIERS
- SANDBAG BERMS
- TEMPORARY STREAM CROSSINGS

OFFSITE VEHICLE TRACKING:

- HAUL ROADS DAMPENED FOR DUST CONTROL
- LOADED HAUL TRUCKS TO BE COVERED WITH TARPAULIN
- EXCESS DIRT ON ROAD REMOVED DAILY

NOTES:

THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE FOLLOWING:

MAINTENANCE AND INSPECTION:

ALL EROSION AND SEDIMENT CONTROLS WILL BE MAINTAINED IN GOOD WORKING ORDER FROM THE BEGINNING OF CONSTRUCTION UNTIL AN ACCEPTABLE VEGETATIVE COVER IS ESTABLISHED. INSPECTION BY THE CONTRACTOR AND ANY NECESSARY REPAIRS SHALL BE PERFORMED ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS AFTER ANY STORM EVENT GREATER THAN 0.5 INCH AS RECORDED BY A NON-FREEZING RAIN GAUGE TO BE LOCATED ON SITE. POTENTIALLY ERODIBLE AREAS, DRAINAGEWAYS, MATERIAL STORAGE, STRUCTURAL DEVICES, CONSTRUCTION ENTRANCES AND EXITS ALONG WITH EROSION AND SEDIMENT CONTROL LOCATIONS ARE EXAMPLES OF SITES THAT NEED TO BE INSPECTED.

WASTE MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF CONSTRUCTION WASTE MATERIAL IS REQUIRED BY THE CONTRACTOR. MATERIALS INCLUDE STOCKPILES, SURPLUS, DEBRIS AND ALL OTHER BY-PRODUCTS FROM THE CONSTRUCTION PROCESS. PRACTICES INCLUDE DISPOSAL, PROPER MATERIALS HANDLING, SPILL PREVENTION AND CLEANUP MEASURES. CONTROLS AND PRACTICES SHALL MEET THE REQUIREMENTS OF ALL FEDERAL, STATE AND LOCAL AGENCIES.

HAZARDOUS MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF HAZARDOUS WASTE MATERIALS IS REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING MANUFACTURER'S RECOMMENDATIONS, STATE AND FEDERAL REGULATIONS TO ENSURE CORRECT HANDLING, DISPOSAL, SPILL PREVENTION AND CLEANUP MEASURES. EXAMPLES INCLUDE BUT ARE NOT LIMITED TO: PAINTS, ACIDS, CLEANING SOLVENTS, CHEMICAL ADDITIVES, CONCRETE CURING COMPOUNDS AND CONTAMINATED SOILS.

GENERAL NOTES:

A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED TO COMPLY WITH THE OKLAHOMA POLLUTION DISCHARGE ELIMINATION SYSTEM (OPDES) REGULATIONS. THIS PLAN IS INITIATED DURING THE DESIGN PHASE, CONFIRMED IN THE PRE-WORK MEETINGS AND AVAILABLE ON THE JOB SITE ALONG WITH COPIES OF THE NOTICE OF INTENT (NOI) FORM AND PERMIT CERTIFICATE THAT HAVE BEEN FILED WITH THE OKLAHOMA DEPARTMENT OF ENVIRONMENTAL QUALITY (ODEQ). THE PLAN MUST BE KEPT CURRENT WITH UP-TO-DATE AMENDMENTS DURING THE PROGRESSION OF THE PROJECT. ALL CONTRACTOR OFF-SITE OPERATIONS ASSOCIATED WITH THE PROJECT MUST BE DOCUMENTED IN THE SWPPP, I.E. BORROW PITS, WORK ROADS, DISPOSAL SITES, ASPHALT/CONCRETE PLANTS, ETC. THE BASIC GOAL OF STORM WATER MANAGEMENT IS TO IMPROVE WATER QUALITY BY REDUCING POLLUTANTS IN STORM WATER DISCHARGES. RUNOFF FROM CONSTRUCTION SITES HAS A POTENTIAL FOR POLLUTION DUE TO EXPOSED SOILS AND THE PRESENCE OF HAZARDOUS MATERIALS USED IN THE CONSTRUCTION PROCESS. THE PREVENTION OF SOIL EROSION, CONTAINMENT OF HAZARDOUS MATERIALS AND/OR THE INTERCEPTION OF THESE POLLUTANTS BEFORE LEAVING THE CONSTRUCTION SITE ARE THE BEST PRACTICES FOR CONTROLLING STORM WATER POLLUTION.

THE FOLLOWING SECTIONS OF THE 2010 OTA STANDARD SPECIFICATIONS SHOULD BE NOTED:

103.05	BONDING REQUIREMENTS
104.10	FINAL CLEAN UP
104.12	CONTRACTOR'S RESPONSIBILITY FOR WORK
104.13	ENVIRONMENTAL PROTECTION
106.08	STORAGE AND HANDLING OF MATERIAL
107.01	LAWS, RULES AND REGULATIONS TO BE OBSERVED
107.20	STORM WATER MANAGEMENT
220	MANAGEMENT OF EROSION, SEDIMENTATION AND STORM WATER POLLUTION PREVENTION AND CONTROL
221	TEMPORARY SEDIMENT CONTROL

IN ADDITION:
 "ODEQ GENERAL PERMIT (OKR10) FOR STORM WATER DISCHARGES FROM CONSTRUCTION ACTIVITIES WITHIN THE STATE OF OKLAHOMA." ODEQ, WATER QUALITY DIVISION, SEPTEMBER 13, 2012.

NOT FOR CONSTRUCTION

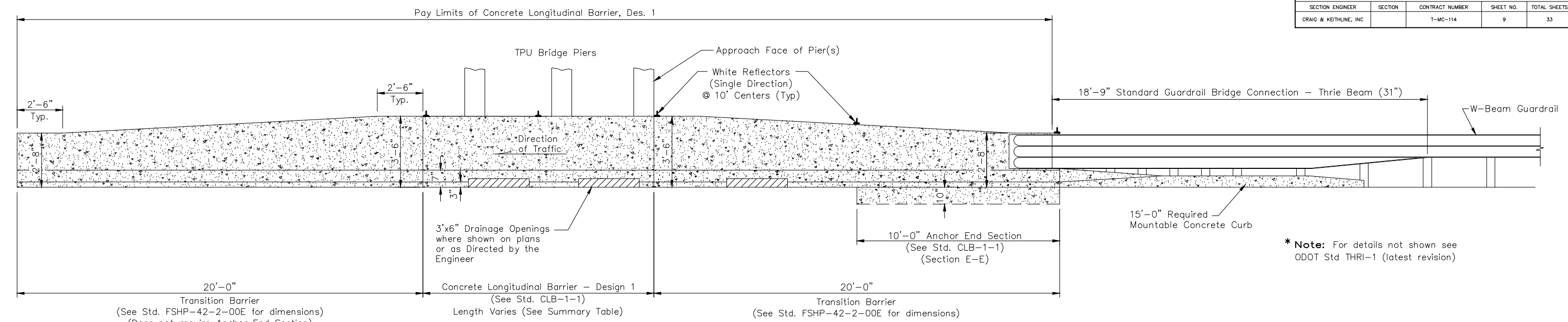
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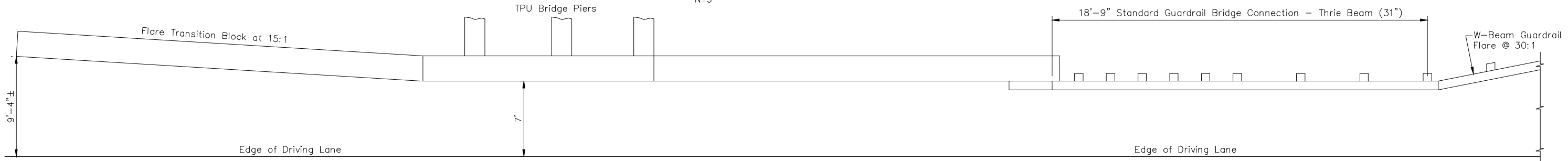
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE	STORM WATER MANAGEMENT PLAN		
PROFILE SCALE HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
VERTICAL			
DESIGNED: OKE	CONTRACT NO. T-MC-114		
DRAWN: CKD	DATE:	SHEET 8 OF 33 SHEETS	
CHECKED:			



* Note: For details not shown see ODOT Std THRI-1 (latest revision)

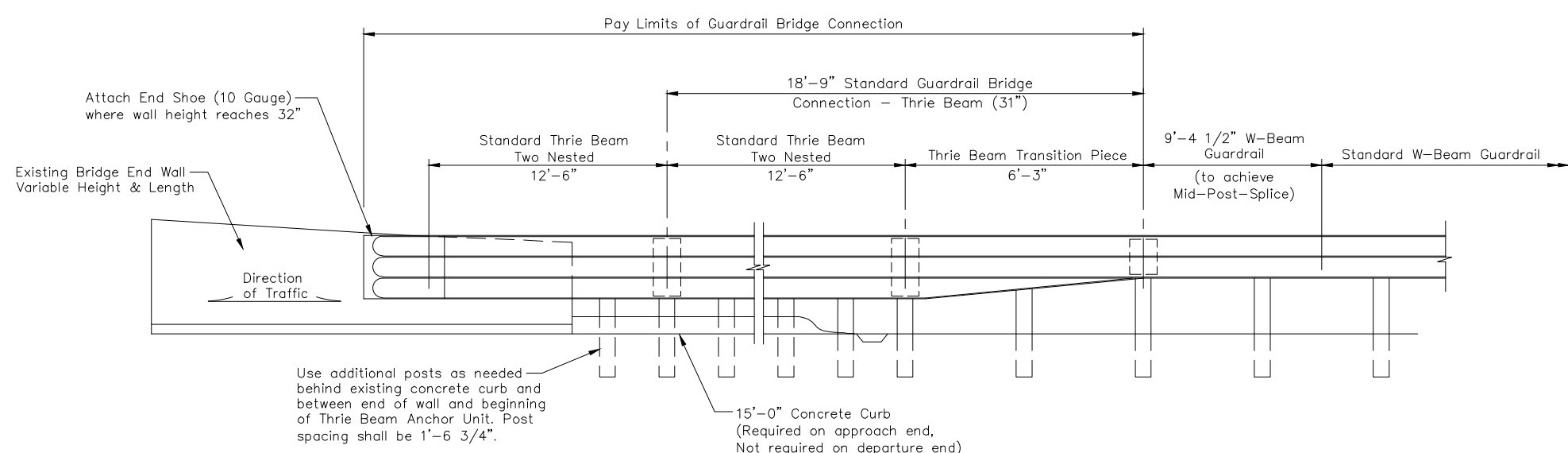
GUARDRAIL BRIDGE CONNECTION - THRIE BEAM (31" SYSTEM) (STANDARD) AT TPU BRIDGE PIERS - ELEVATION

TPU 60.40
NTS



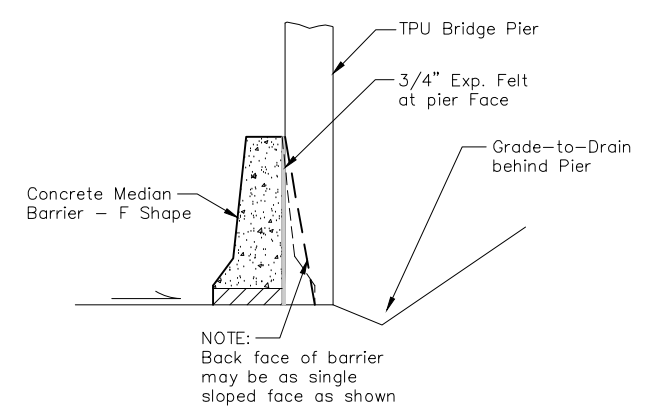
GUARDRAIL BRIDGE CONNECTION - THRIE BEAM (31" SYSTEM) (STANDARD) AT TPU BRIDGE PIERS - PLAN

NTS



GUARDRAIL BRIDGE CONNECTION - THRIE BEAM (31" SYSTEM) (MODIFIED) AT EXISTING MAINLINE BRIDGE PARAPETS - ELEVATION

FLARE RATE 30:1 APPROACH END
15:1 TRAILING END
TPO 61.10, 61.20, 61.40
NTS



END SECTION AT PIER FACE (TYPICAL)

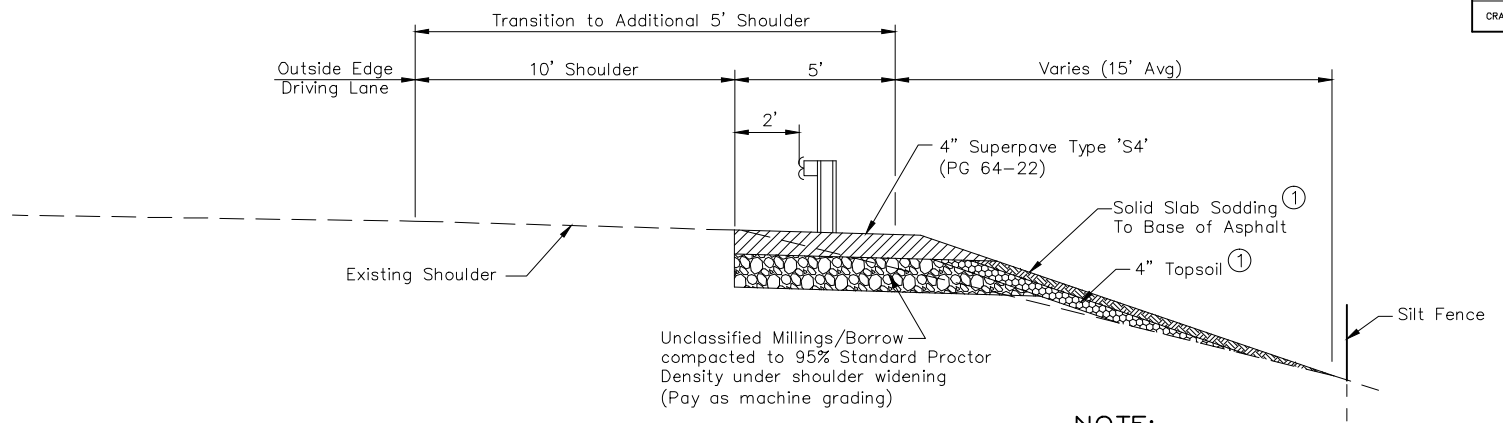
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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE	GUARDRAIL BRIDGE CONNECTION AND PIER PROTECTION BARRIER DETAILS		
PROFILE SCALE			
HORIZONTAL			
VERTICAL			
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED			SHEET 9 OF 33 SHEETS



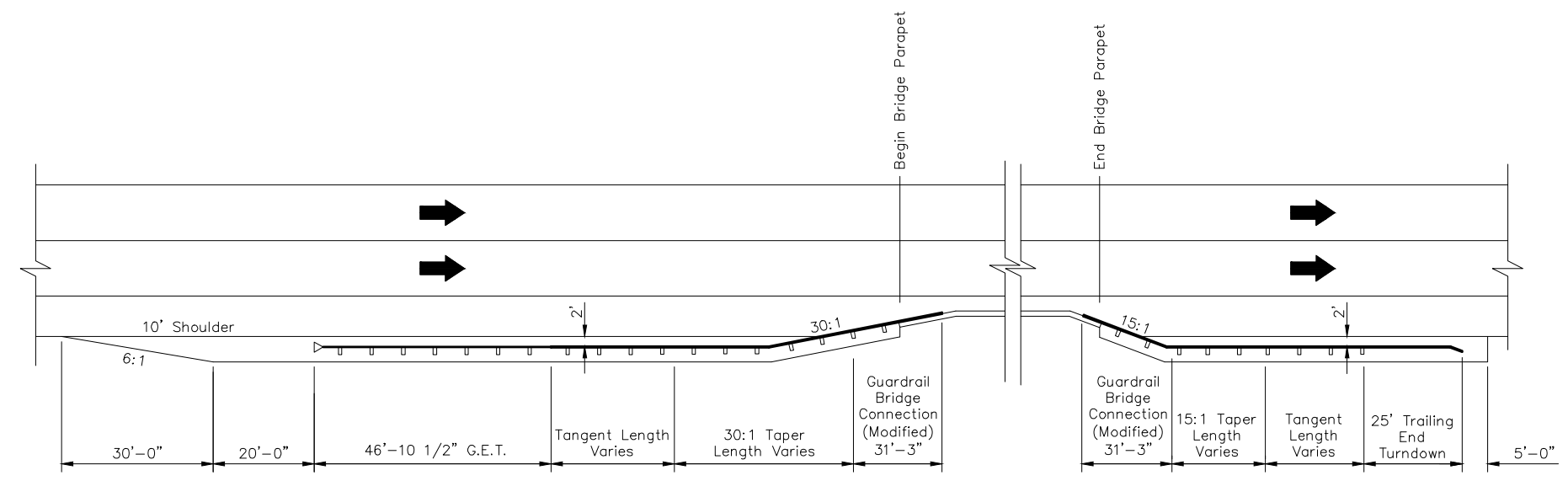
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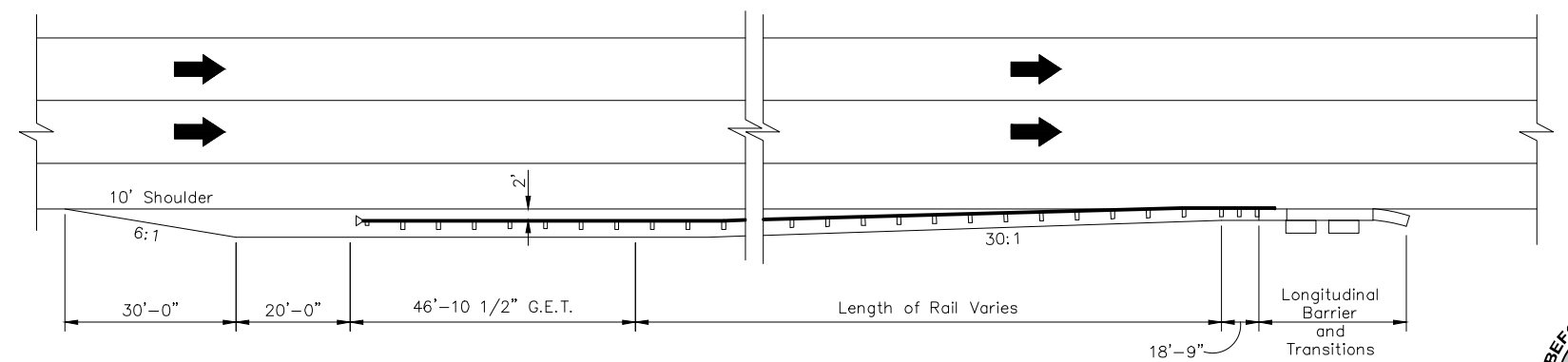


**GUARDRAIL WIDENING DETAIL
(NON FULL-DEPTH REPAIR AREAS)**
(Distance from edge lane to face-of-rail 12' in tangent sections)
N.T.S.

NOTE:
1. Topsoil and Solid Slab Sod to be used for shouldering up as required. Contractor to shoulder behind all guardrails to the top of the shoulder as directed by the Engineer. Estimated at 15'-0" wide, 3:1 Max. Slope.



TYPICAL TPO BRIDGE PARAPET PROTECTION WITH TRANSITION
TPO 61.10, 61.20, 61.40
(ALL REFLECTORS WHITE)
N.T.S.



TYPICAL TPU PIER PROTECTION WITH TRANSITION
TPU 60.40
(ALL REFLECTORS WHITE)
N.T.S.

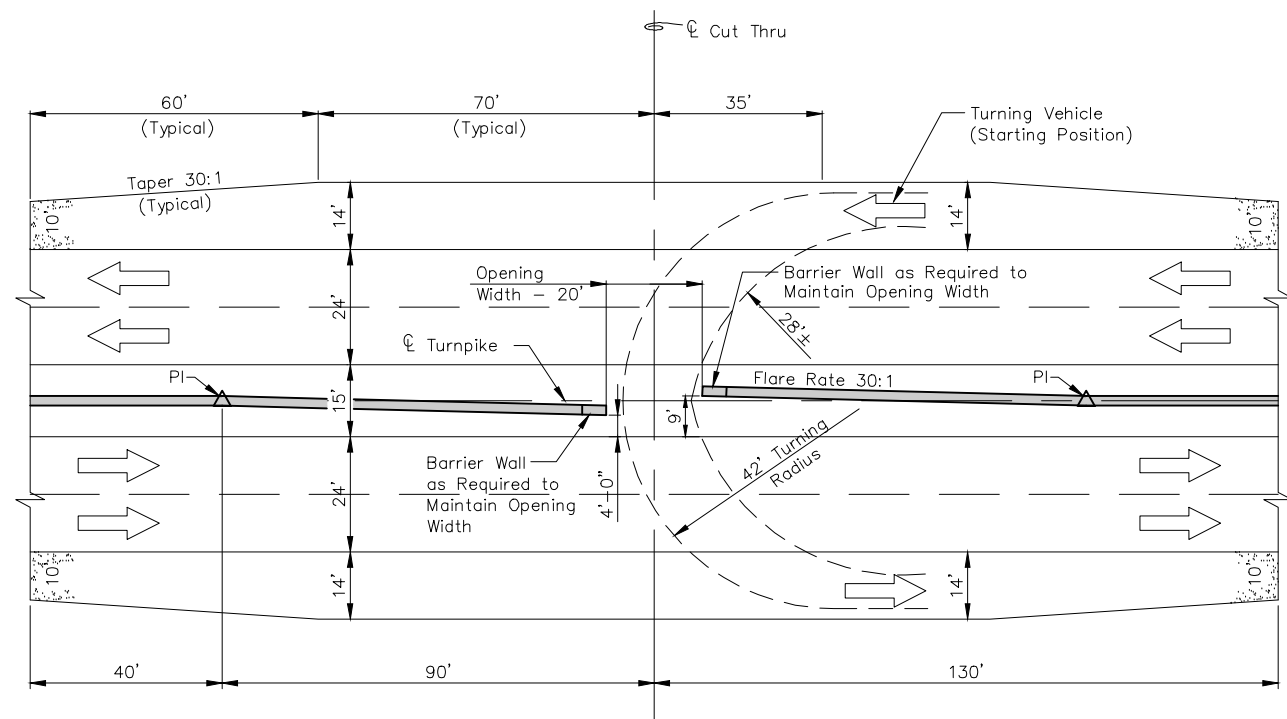
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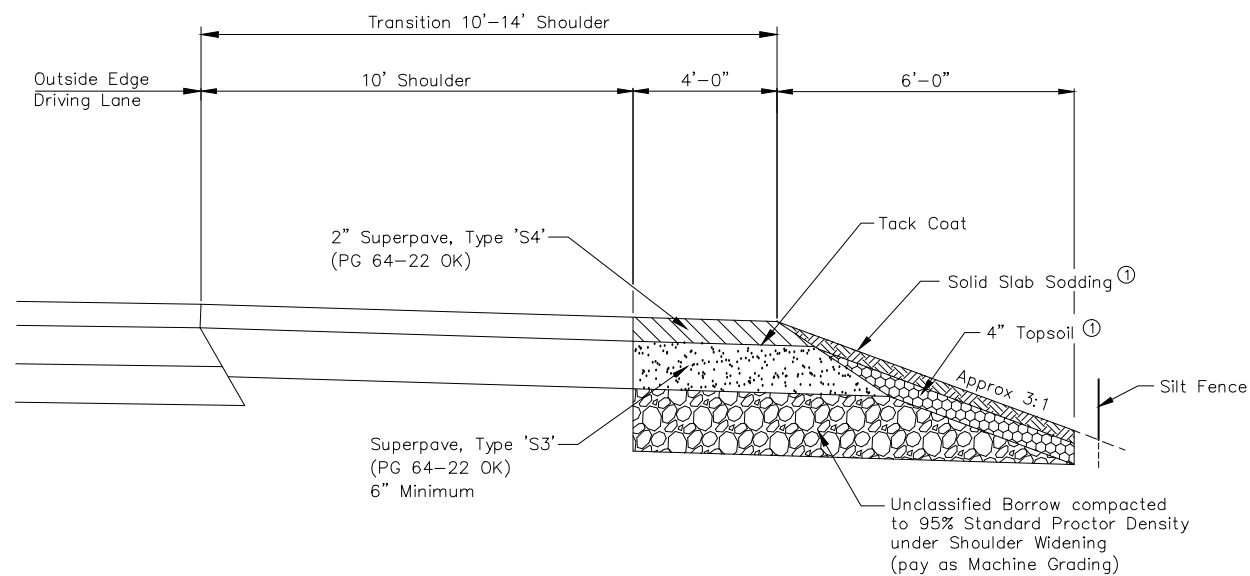
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PLAN SCALE	GUARDRAIL DETAILS		
PROFILE SCALE			
HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	SECTION	
VERTICAL			
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 10 OF 33 SHEETS	

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**EMERGENCY VEHICLE
TURNAROUND LOCATIONS**
STATION 1143+00

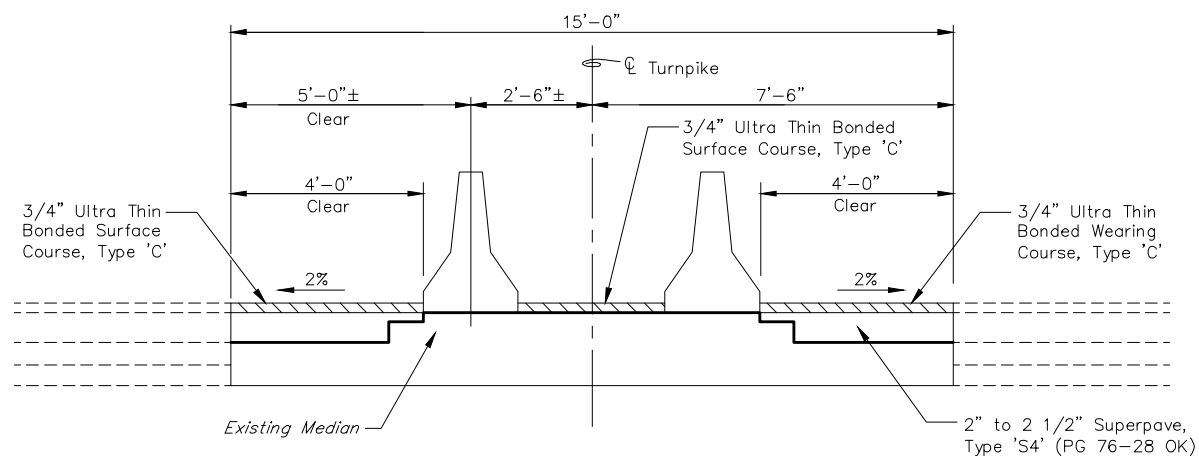


**PLAN VIEW
EMERGENCY VEHICLE TURNAROUND OVERLAY**
N.T.S.



**TYPICAL SECTION - SHOULDER WIDENING
AT EXISTING TURNAROUNDS**
N.T.S.

NOTE:
1. Topsoil and Solid Slab Sod to be used for Shouldering Up as required. Contractor to Shoulder behind all Widening to the Top of the Shoulder as directed by the Engineer. Estimated at 15'-0" Wide, 3:1 Max. Slope.



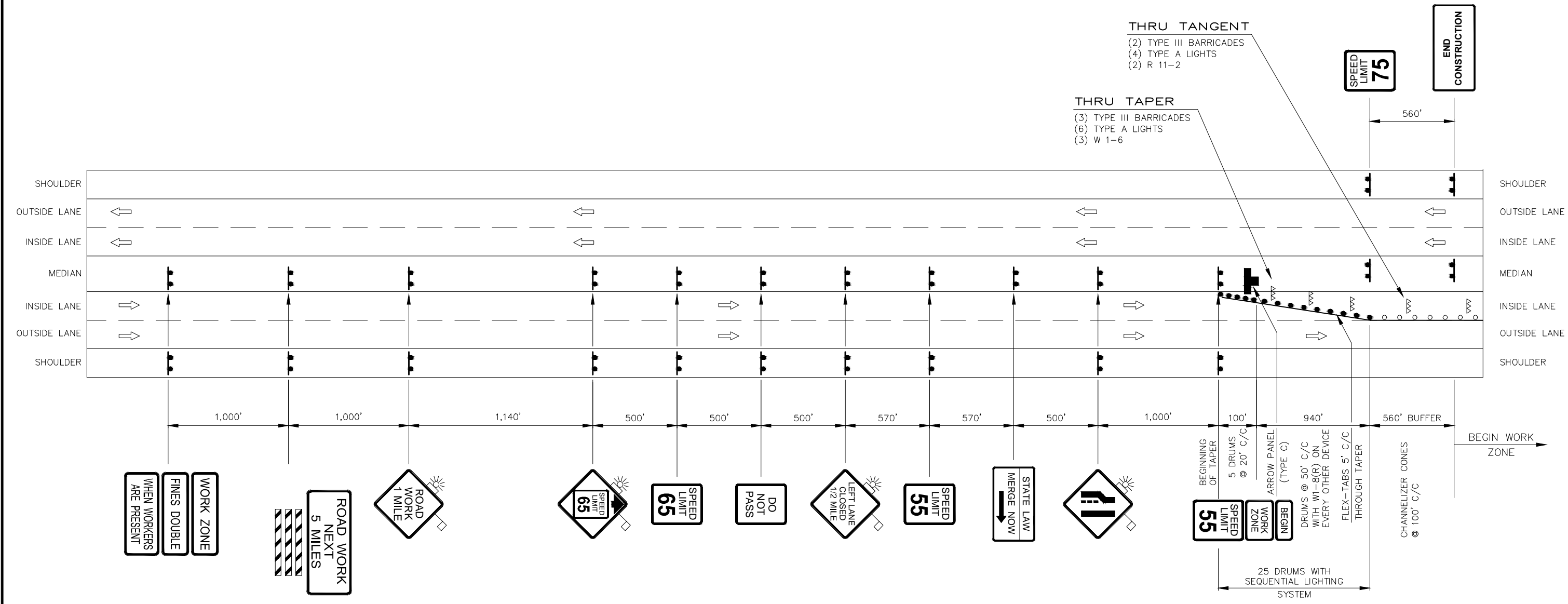
**MEDIAN SECTION
EMERGENCY VEHICLE TURNAROUND OVERLAY**
N.T.S.

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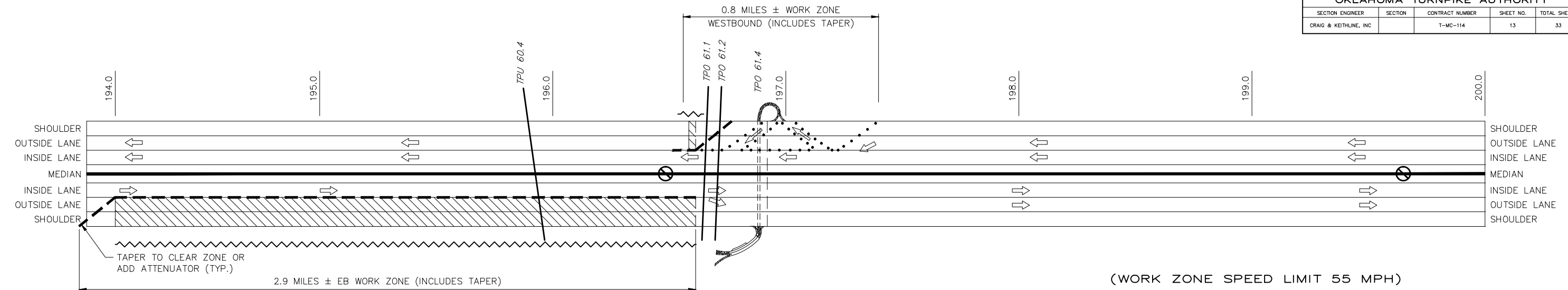
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NOT TO SCALE	EMERGENCY VEHICLE TURNAROUND DETAILS		
PROFILE SCALE HORIZONTAL NA VERTICAL NA		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	SECTION
DESIGNED DRAWN CHECKED	CKE CKD	CONTRACT NO. T-MC-114	DATE: SHEET 11 OF 33 SHEETS



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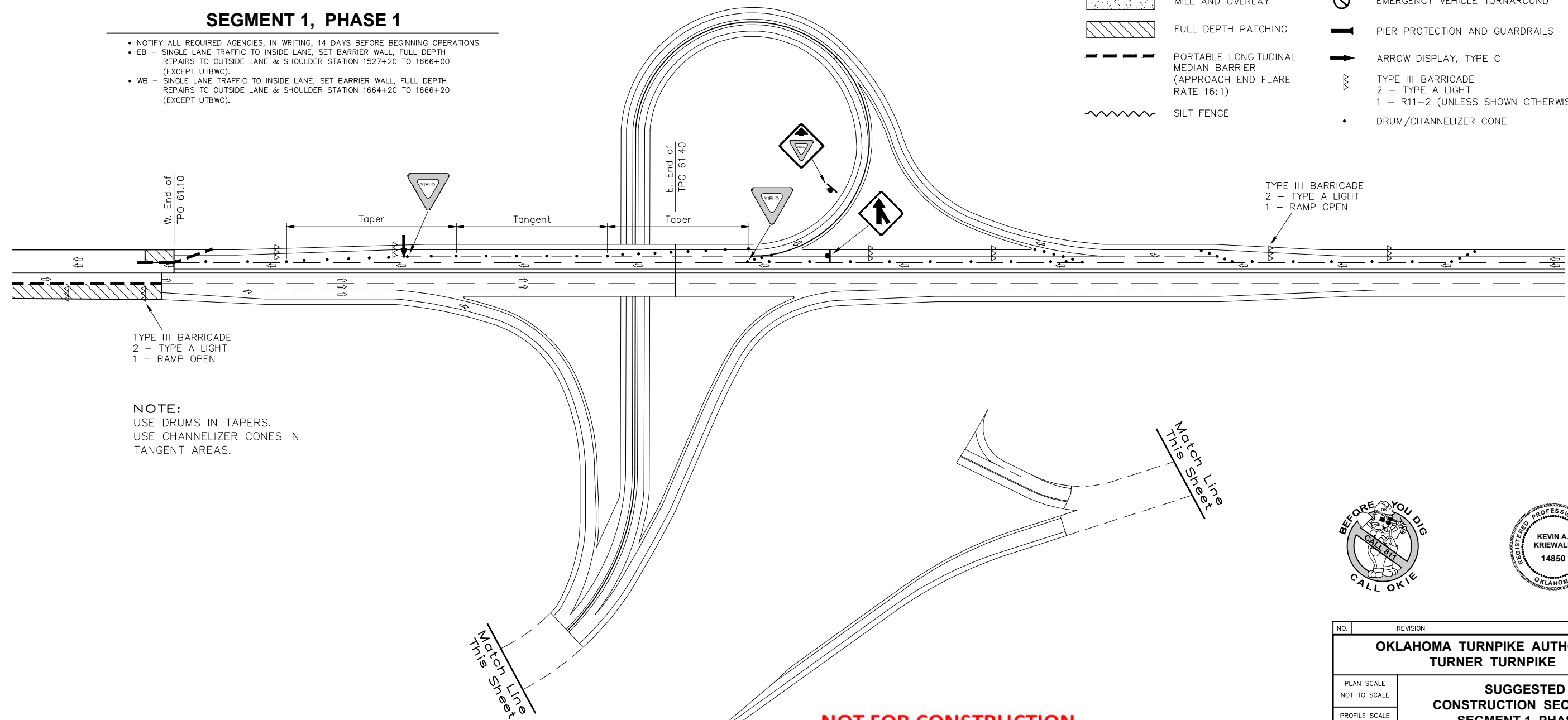
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE	OKLAHOMA 75 MPH LANE REDUCTION TO 55 MPH ADVANCE SIGNING		
PROFILE SCALE	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
HORIZONTAL			
VERTICAL	SECTION		
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 12 OF 33 SHEETS	



SEGMENT 1, PHASE 1

- NOTIFY ALL REQUIRED AGENCIES, IN WRITING, 14 DAYS BEFORE BEGINNING OPERATIONS
- EB - SINGLE LANE TRAFFIC TO INSIDE LANE, SET BARRIER WALL, FULL DEPTH REPAIRS TO OUTSIDE LANE & SHOULDER STATION 1527+20 TO 1666+00 (EXCEPT UTBC).
- WB - SINGLE LANE TRAFFIC TO INSIDE LANE, SET BARRIER WALL, FULL DEPTH REPAIRS TO OUTSIDE LANE & SHOULDER STATION 1664+20 TO 1666+20 (EXCEPT UTBC).

- (WORK ZONE SPEED LIMIT 55 MPH)
- MILL AND OVERLAY
 - FULL DEPTH PATCHING
 - PORTABLE LONGITUDINAL MEDIAN BARRIER (APPROACH END FLARE RATE 16:1)
 - SILT FENCE
 - EMERGENCY VEHICLE TURNAROUND
 - PIER PROTECTION AND GUARDRAILS
 - ARROW DISPLAY, TYPE C
 - TYPE III BARRICADE
2 - TYPE A LIGHT
1 - R11-2 (UNLESS SHOWN OTHERWISE)
 - DRUM/CHANNELIZER CONE

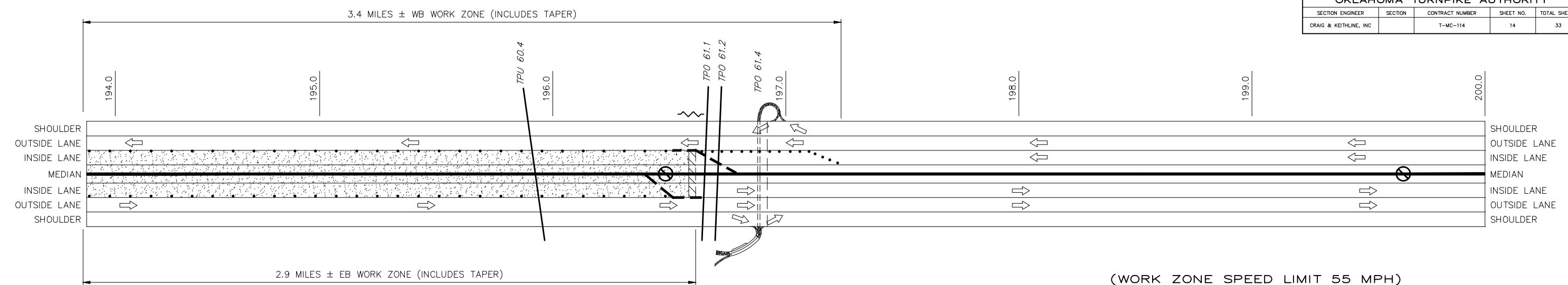


NOTE:
USE DRUMS IN TAPERS.
USE CHANNELIZER CONES IN TANGENT AREAS.



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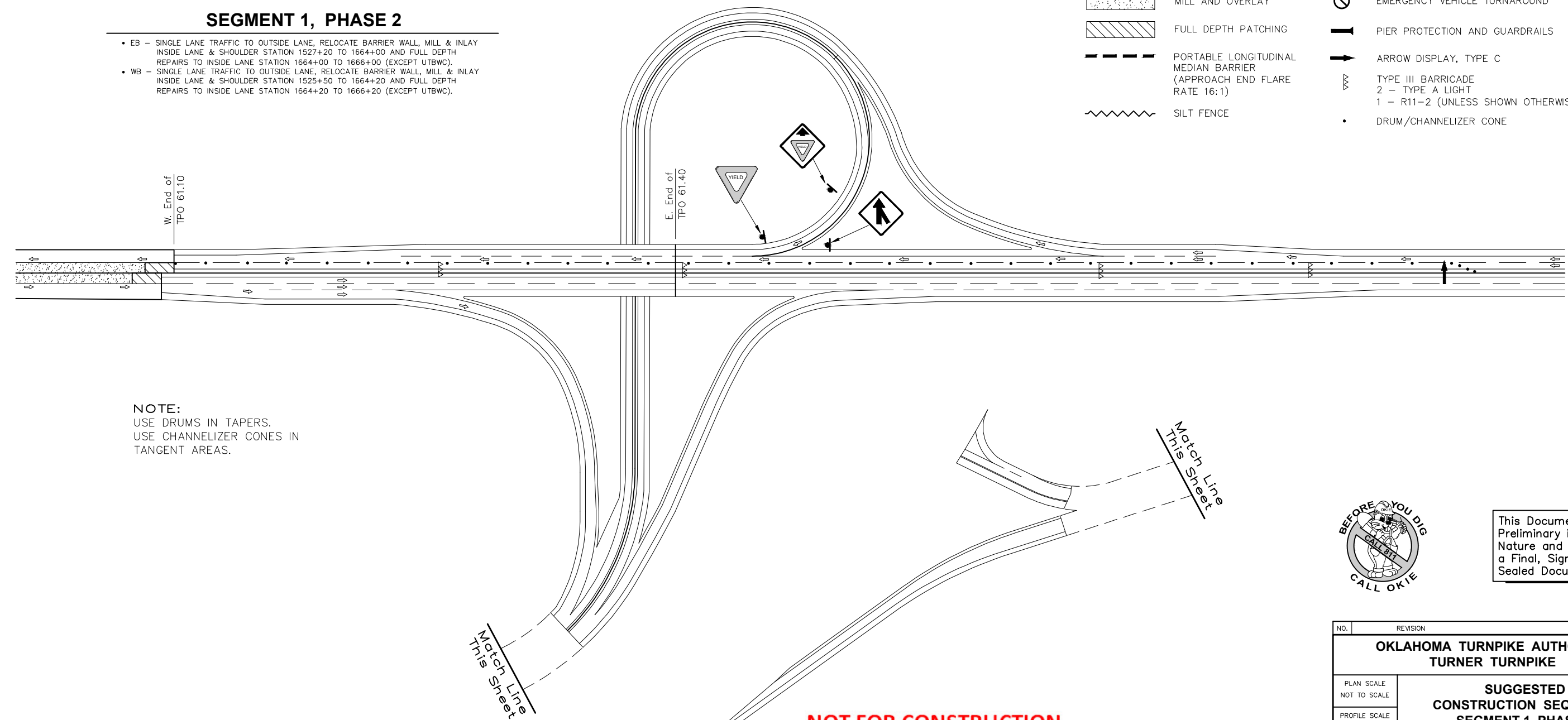
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OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NOT TO SCALE	SUGGESTED CONSTRUCTION SEQUENCE SEGMENT 1, PHASE 1		
PROFILE SCALE HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
VERTICAL			
DESIGNED: CKE	CONTRACT NO. T-MC-114	SECTION	
DRAWN: CKD	DATE:	SHEET 13 OF 33 SHEETS	
CHECKED:			



SEGMENT 1, PHASE 2

- EB - SINGLE LANE TRAFFIC TO OUTSIDE LANE, RELOCATE BARRIER WALL, MILL & INLAY INSIDE LANE & SHOULDER STATION 1527+20 TO 1664+00 AND FULL DEPTH REPAIRS TO INSIDE LANE STATION 1664+00 TO 1666+00 (EXCEPT UTBWC).
- WB - SINGLE LANE TRAFFIC TO OUTSIDE LANE, RELOCATE BARRIER WALL, MILL & INLAY INSIDE LANE & SHOULDER STATION 1525+50 TO 1664+20 AND FULL DEPTH REPAIRS TO INSIDE LANE STATION 1664+20 TO 1666+20 (EXCEPT UTBWC).

- (WORK ZONE SPEED LIMIT 55 MPH)
- MILL AND OVERLAY
 - FULL DEPTH PATCHING
 - PORTABLE LONGITUDINAL MEDIAN BARRIER (APPROACH END FLARE RATE 16:1)
 - SILT FENCE
 - EMERGENCY VEHICLE TURNAROUND
 - PIER PROTECTION AND GUARDRAILS
 - ARROW DISPLAY, TYPE C
 - TYPE III BARRICADE
2 - TYPE A LIGHT
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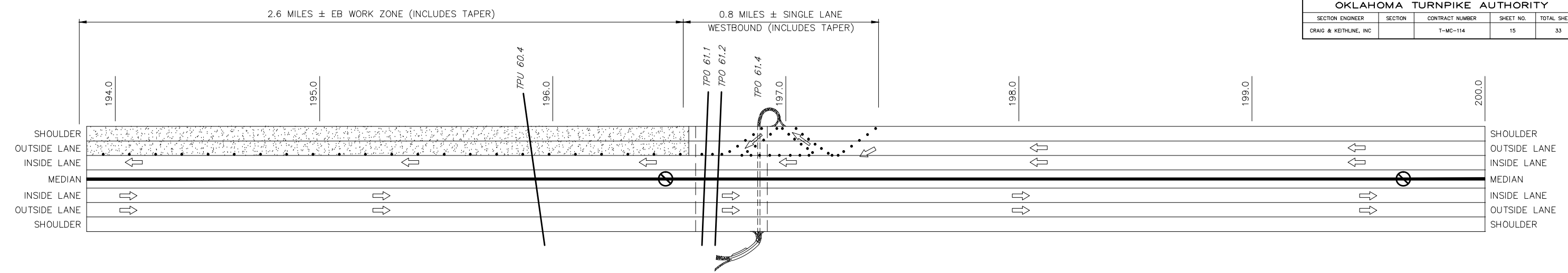
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This Document is Preliminary in Nature and is Not a Final, Signed and Sealed Document

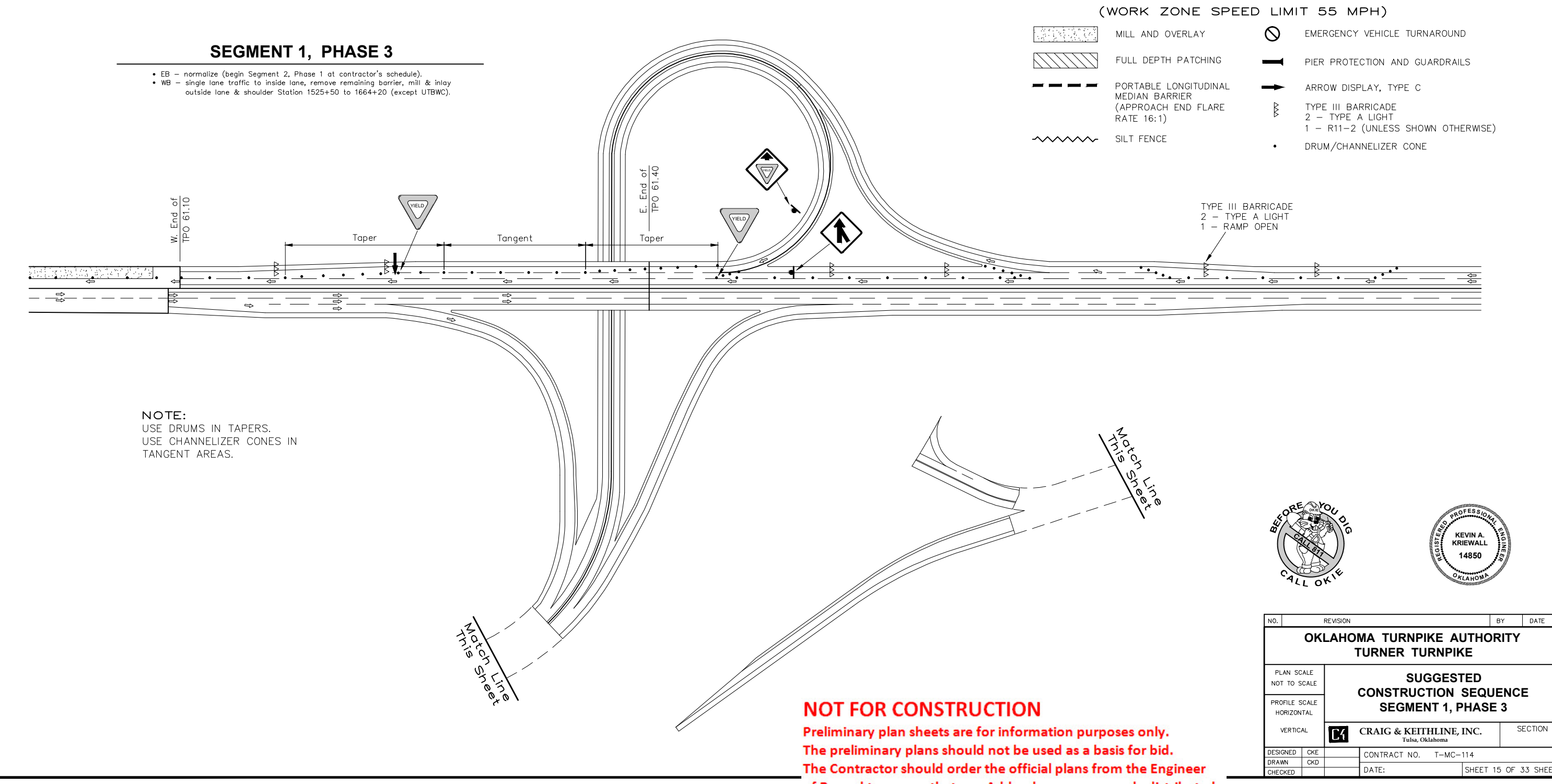
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PLAN SCALE NOT TO SCALE	SUGGESTED CONSTRUCTION SEQUENCE SEGMENT 1, PHASE 2		
PROFILE SCALE HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
VERTICAL			
DESIGNED: CKE	CONTRACT NO. T-MC-114		SECTION
DRAWN: CKD	DATE:		SHEET 14 OF 33 SHEETS
CHECKED:			



SEGMENT 1, PHASE 3

- EB - normalize (begin Segment 2, Phase 1 at contractor's schedule).
- WB - single lane traffic to inside lane, remove remaining barrier, mill & inlay outside lane & shoulder Station 1525+50 to 1664+20 (except UTBWC).



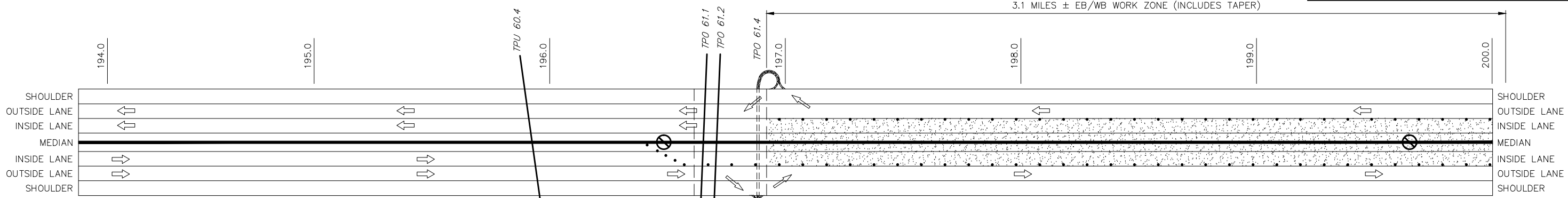
- (WORK ZONE SPEED LIMIT 55 MPH)
- MILL AND OVERLAY
 - FULL DEPTH PATCHING
 - PORTABLE LONGITUDINAL MEDIAN BARRIER (APPROACH END FLARE RATE 16:1)
 - SILT FENCE
 - EMERGENCY VEHICLE TURNAROUND
 - PIER PROTECTION AND GUARDRAILS
 - ARROW DISPLAY, TYPE C
 - TYPE III BARRICADE
2 - TYPE A LIGHT
1 - R11-2 (UNLESS SHOWN OTHERWISE)
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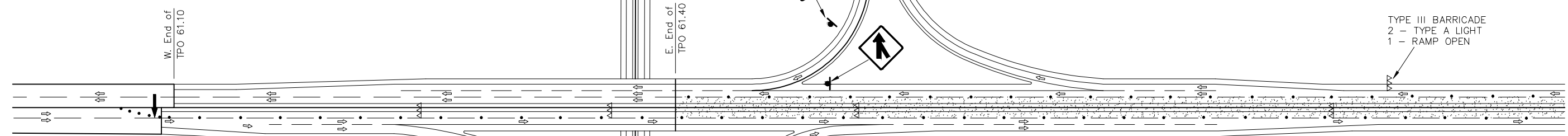
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OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NOT TO SCALE	SUGGESTED CONSTRUCTION SEQUENCE SEGMENT 1, PHASE 3		
PROFILE SCALE HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
VERTICAL			
DESIGNED: CKE	CONTRACT NO. T-MC-114		SECTION
DRAWN: CKD	DATE:		SHEET 15 OF 33 SHEETS
CHECKED:			



SEGMENT 2, PHASE 1

- EB - single lane traffic to outside lane, mill & inlay inside lane & shoulder Station 1680+10 to 1845+95 (except UTBWC).
- WB - single lane traffic to outside lane, mill & inlay inside lane & shoulder Station 1680+10 to 1845+95 (except UTBWC).

- (WORK ZONE SPEED LIMIT 55 MPH)
- MILL AND OVERLAY
 - FULL DEPTH PATCHING
 - PORTABLE LONGITUDINAL MEDIAN BARRIER (APPROACH END FLARE RATE 16:1)
 - SILT FENCE
 - EMERGENCY VEHICLE TURNAROUND
 - PIER PROTECTION AND GUARDRAILS
 - ARROW DISPLAY, TYPE C
 - TYPE III BARRICADE
2 - TYPE A LIGHT
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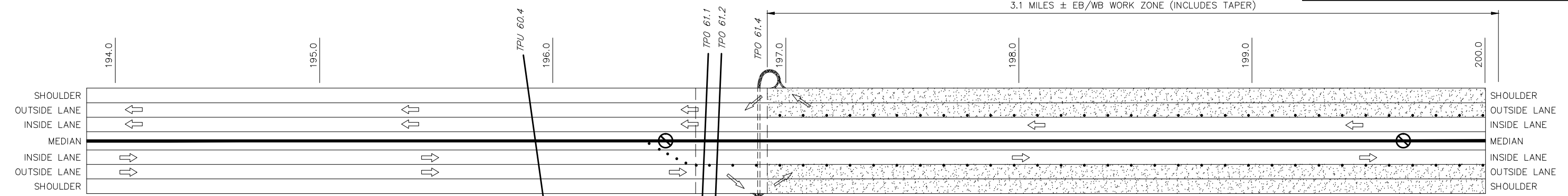


NOTE:
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USE CHANNELIZER CONES IN TANGENT AREAS.

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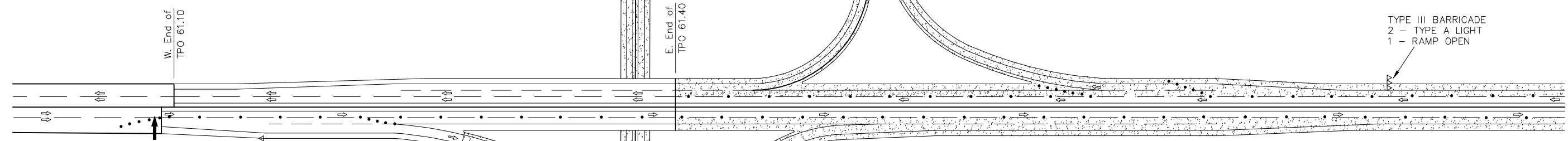
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OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NOT TO SCALE	SUGGESTED CONSTRUCTION SEQUENCE SEGMENT 2, PHASE 1		
PROFILE SCALE HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
VERTICAL			
DESIGNED: CKE	CONTRACT NO. T-MC-114		SECTION
DRAWN: CKD	DATE:		SHEET 16 OF 33 SHEETS
CHECKED:			



SEGMENT 2, PHASE 2

- EB - SINGLE LANE TRAFFIC TO INSIDE LANE, MILL & INLAY OUTSIDE LANE & SHOULDER STATION 1680+10 TO 1845+95 (EXCEPT UTBWC).
- WB - SINGLE LANE TRAFFIC TO INSIDE LANE, MILL & INLAY OUTSIDE LANE & SHOULDER STATION 1680+10 TO 1845+95 (EXCEPT UTBWC).
- RAMPS - MILL & INLAY RAMPS HALF-AT-A-TIME UNDER TRAFFIC USING CHANNELIZER CONES AND FLAGMEN. LIMIT REQUIRED CLOSURES TO 15 MINUTE INTERVALS.

- (WORK ZONE SPEED LIMIT 55 MPH)
- MILL AND OVERLAY
 - FULL DEPTH PATCHING
 - PORTABLE LONGITUDINAL MEDIAN BARRIER (APPROACH END FLARE RATE 16:1)
 - SILT FENCE
 - EMERGENCY VEHICLE TURNAROUND
 - PIER PROTECTION AND GUARDRAILS
 - ARROW DISPLAY, TYPE C
 - TYPE III BARRICADE
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 - DRUM/CHANNELIZER CONE



NOTE:
USE DRUMS IN TAPERS.
USE CHANNELIZER CONES IN TANGENT AREAS.

SEGMENT 1 & 2, FINAL PHASE

- NORMALIZE EB AND WB, INSTALL UTBWC AND PERMANENT PAVEMENT MARKINGS THROUGHOUT.

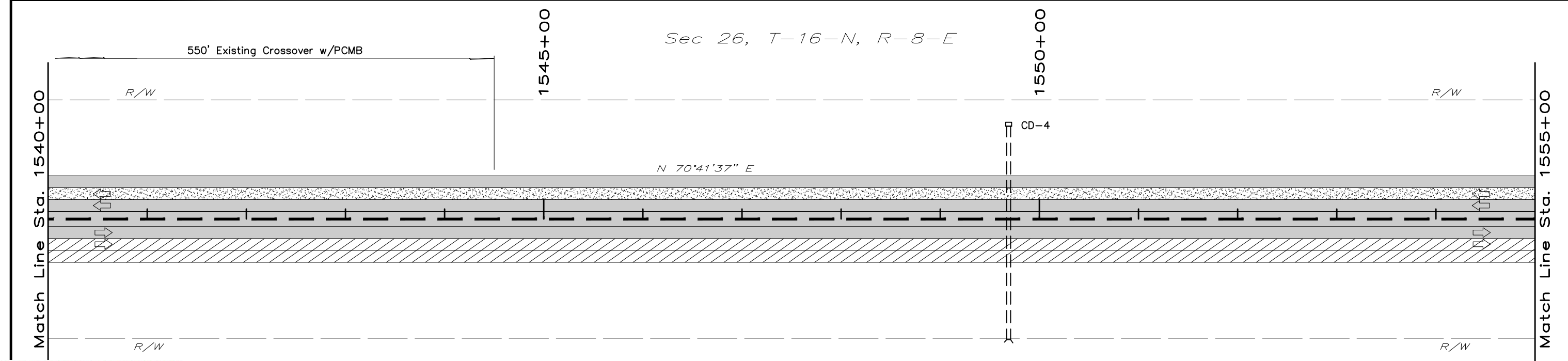
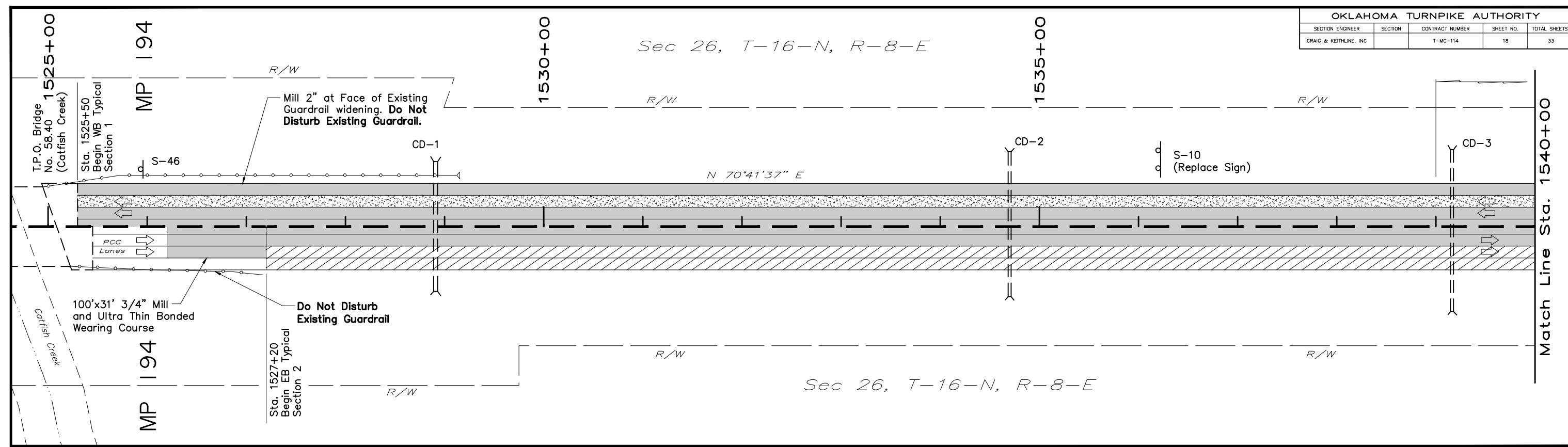
Match Line
This Sheet

Match Line
This Sheet

NOT FOR CONSTRUCTION
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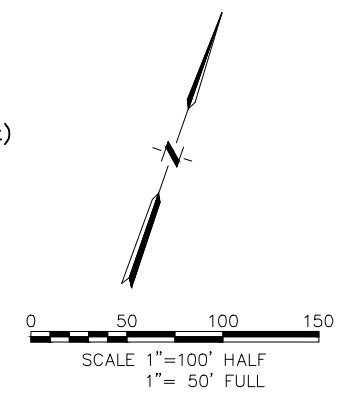
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE NOT TO SCALE	SUGGESTED CONSTRUCTION SEQUENCE SEGMENT 2, PHASE 2		
PROFILE SCALE HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		
VERTICAL			
DESIGNED DRAWN CHECKED	CKE CKD CKD	CONTRACT NO. DATE:	T-MC-114 SHEET 17 OF 33 SHEETS



NOT FOR CONSTRUCTION
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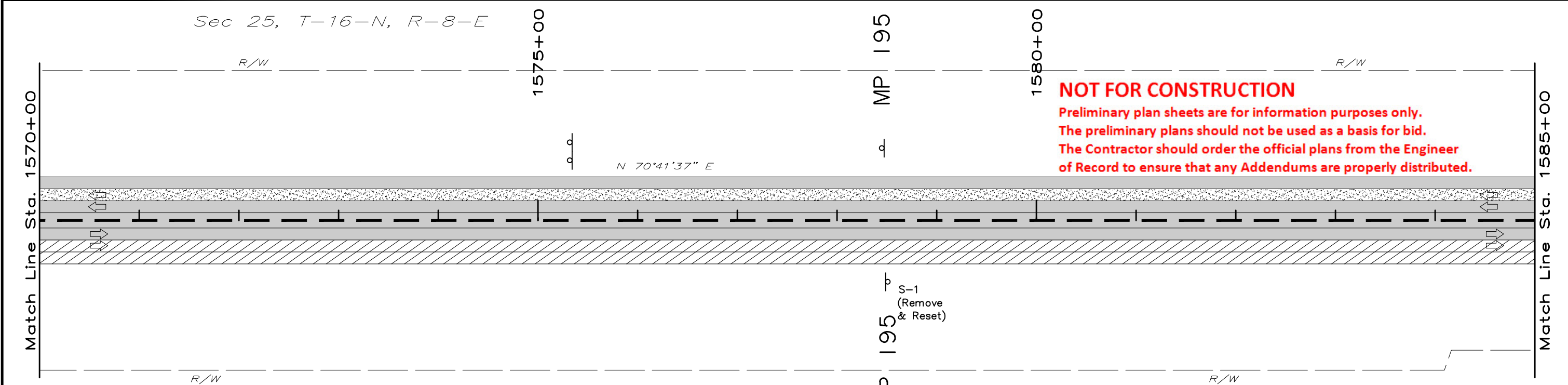
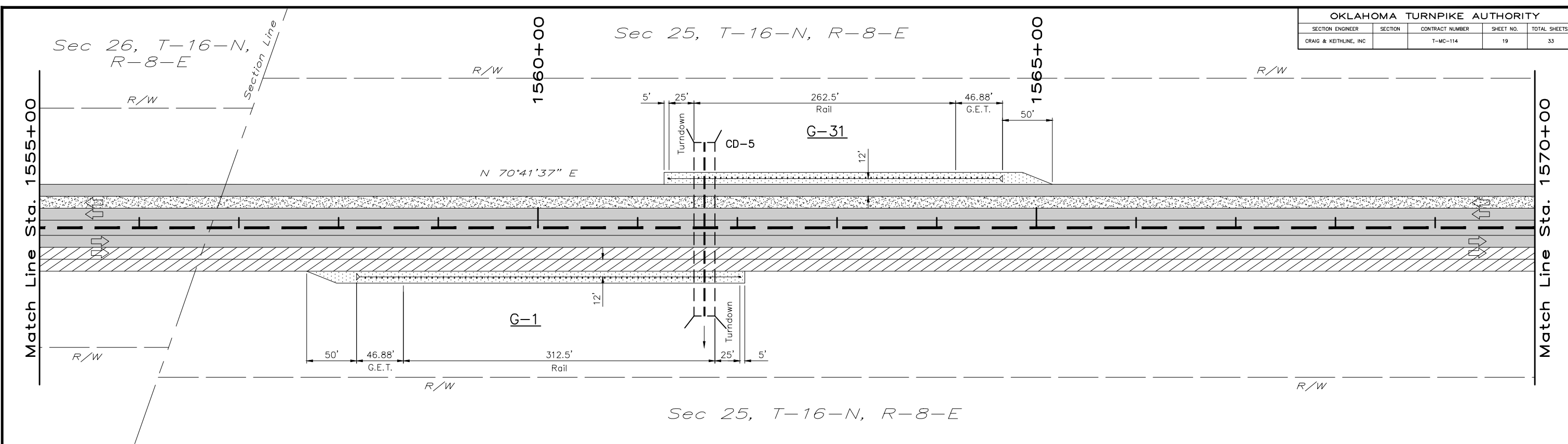
Note:
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- LEGEND**
- Shallow Mill & Overlay
 - Deep Mill & Overlay
 - Full Depth Reconstruction
 - Guardrail Widening
 - Existing Median Barrier
 - Sign
 - Sign to be Replaced/Added
 - Guardrail (Proposed)
 - Guardrail (Type B Anchor Unit)
 - Guardrail (G.E.T. Anchor Unit)
 - Headwall / End Treatment
 - Existing Edge Drain Outlet
 - New Edge Drain Outlet
 - Cross Drain
 - Side Drain



NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1527+19.96 TO STA. 1555+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 18 OF 33 SHEETS	

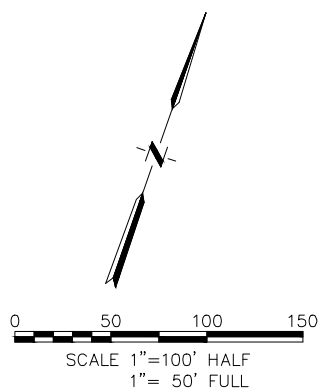
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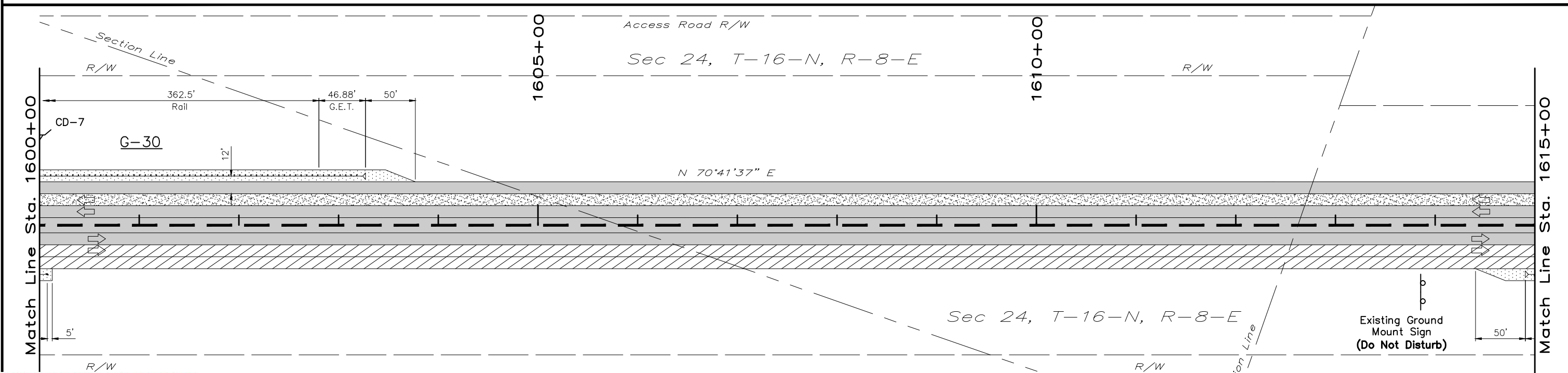
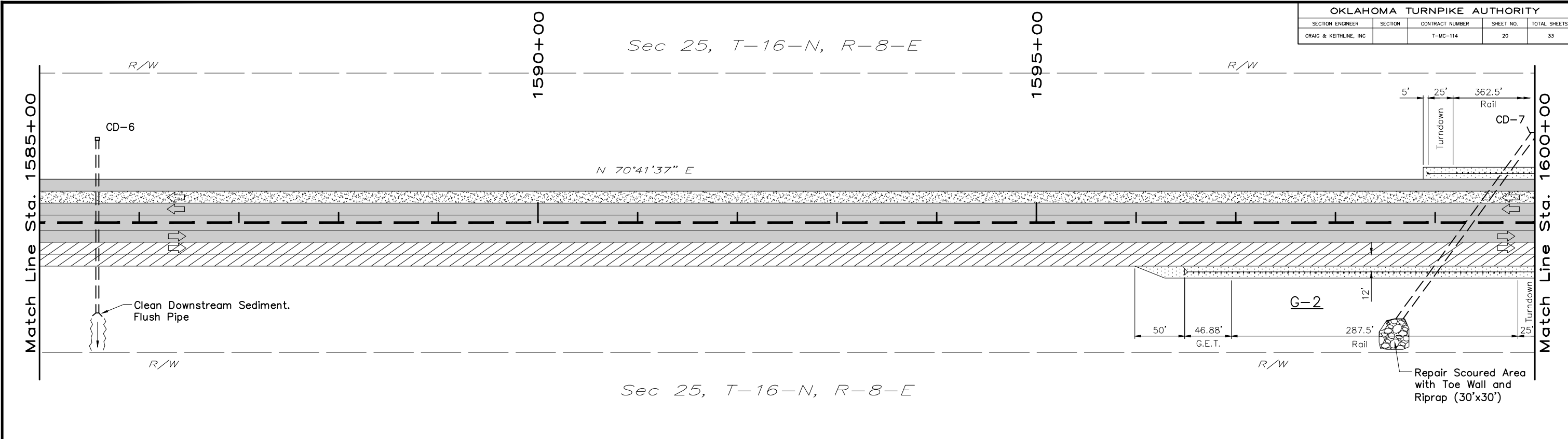
Note:
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- LEGEND**
- Shallow Mill & Overlay
 - Deep Mill & Overlay
 - Full Depth Reconstruction
 - Guardrail Widening
 - Existing Median Barrier
 - Sign
 - Sign to be Replaced/Added
 - Guardrail (Proposed)
 - Guardrail (Type B Anchor Unit)
 - Guardrail (G.E.T. Anchor Unit)
 - Headwall / End Treatment
 - Existing Edge Drain Outlet
 - New Edge Drain Outlet
 - Cross Drain
 - Side Drain



NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1555+00 TO STA. 1585+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 19 OF 33 SHEETS	

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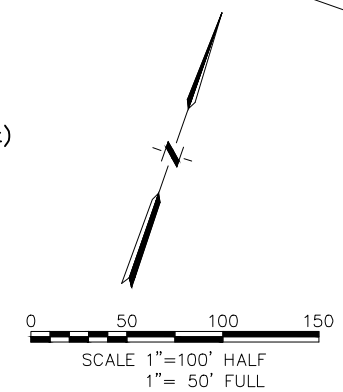
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LEGEND

- | | | | |
|--|---------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |



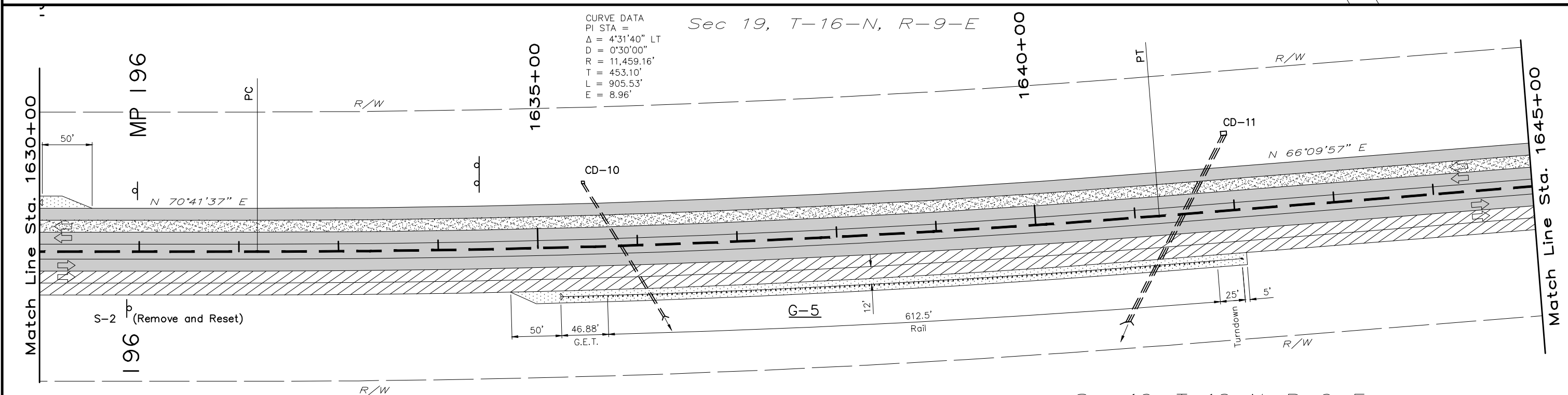
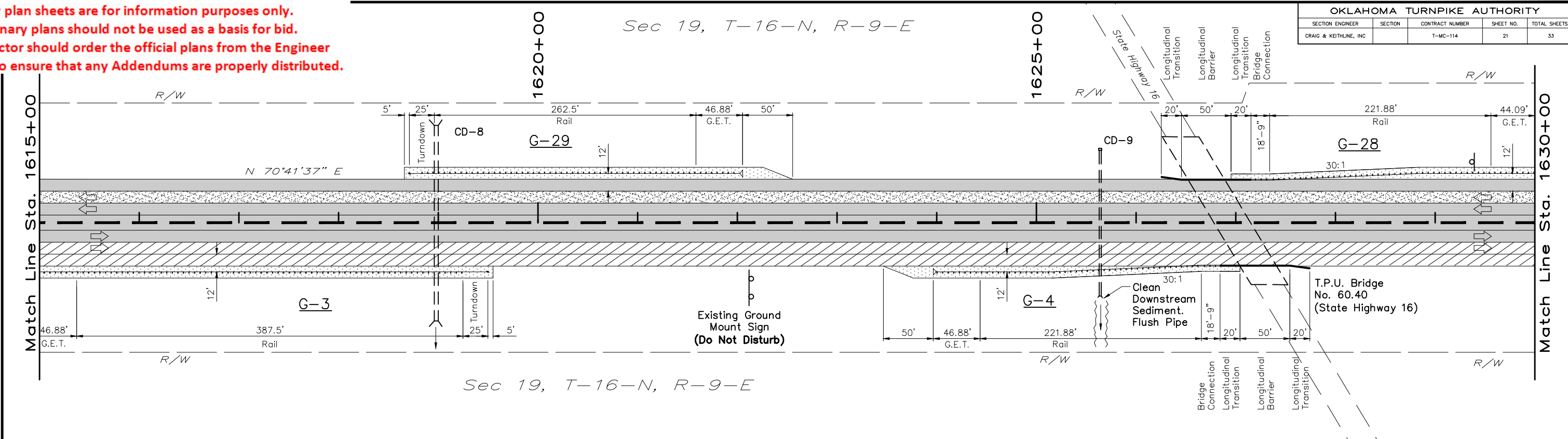
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1585+00 TO STA. 1615+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 20 OF 33 SHEETS	

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NOT FOR CONSTRUCTION

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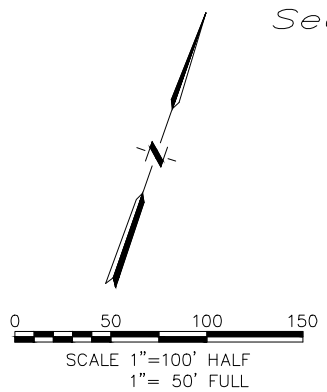
OKLAHOMA TURNPIKE AUTHORITY				
SECTION ENGINEER	SECTION	CONTRACT NUMBER	SHEET NO.	TOTAL SHEETS
CRAIG & KEITHLINE, INC.		T-MC-114	21	33



Note:
 Plan sheets are a schematic representation based on existing mileposts. Actual centerline measured length has been used for all quantity calculations. Delineator placement shall be adjusted at each existing milepost.

LEGEND

	Shallow Mill & Overlay		Guardrail (Proposed)
	Deep Mill & Overlay		Guardrail (Type B Anchor Unit)
	Full Depth Reconstruction		Guardrail (G.E.T. Anchor Unit)
	Guardrail Widening		Headwall / End Treatment
	Existing Median Barrier		Existing Edge Drain Outlet
	Sign		New Edge Drain Outlet
	Sign to be Replaced/Added		Cross Drain
			Side Drain



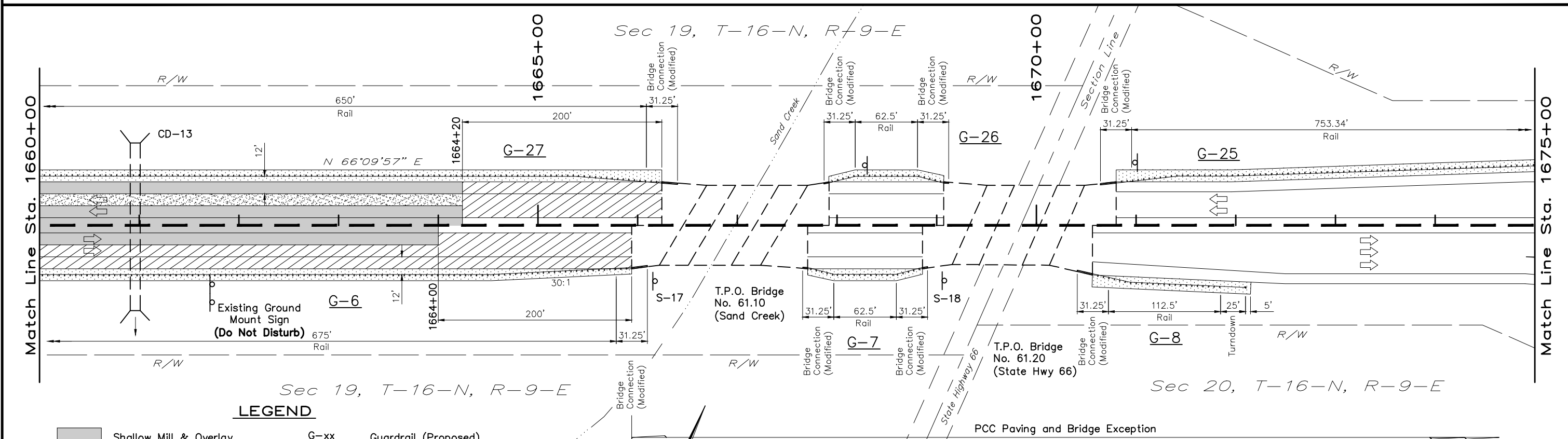
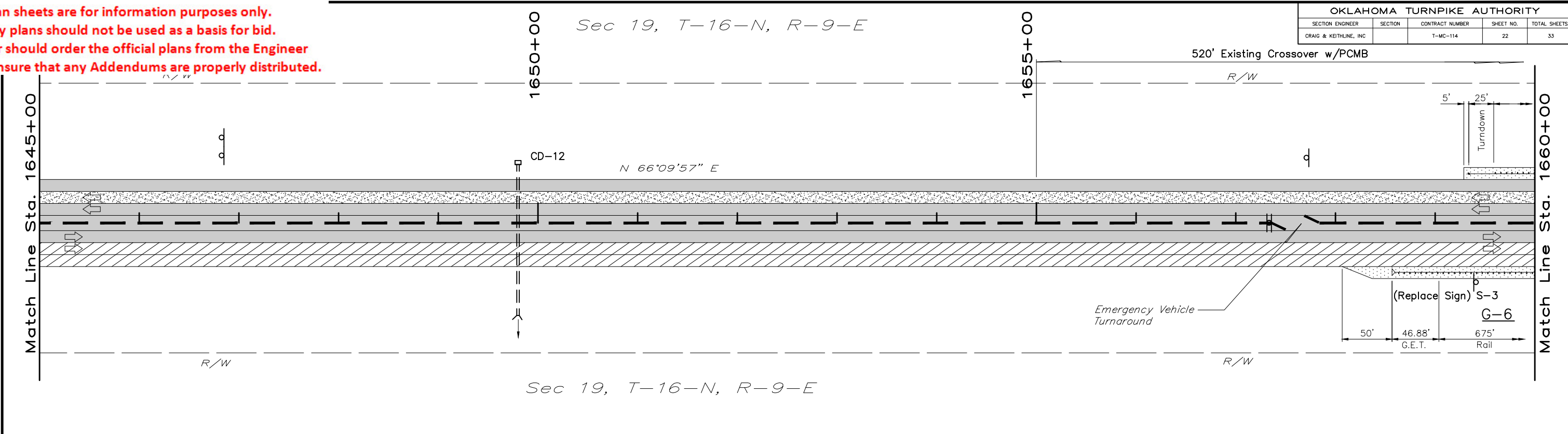
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1615+00 TO STA. 1645+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 21 OF 33 SHEETS	

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NOT FOR CONSTRUCTION

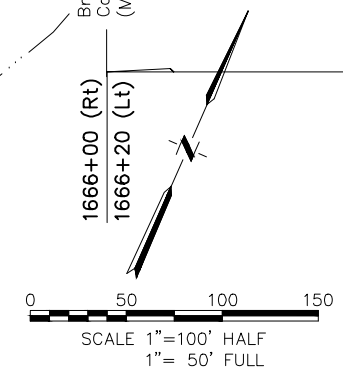
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OKLAHOMA TURNPIKE AUTHORITY				
SECTION ENGINEER	SECTION	CONTRACT NUMBER	SHEET NO.	TOTAL SHEETS
CRAIG & KEITHLINE, INC		T-MC-114	22	33



LEGEND

- | | | | |
|--|--------------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | S-XX Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |

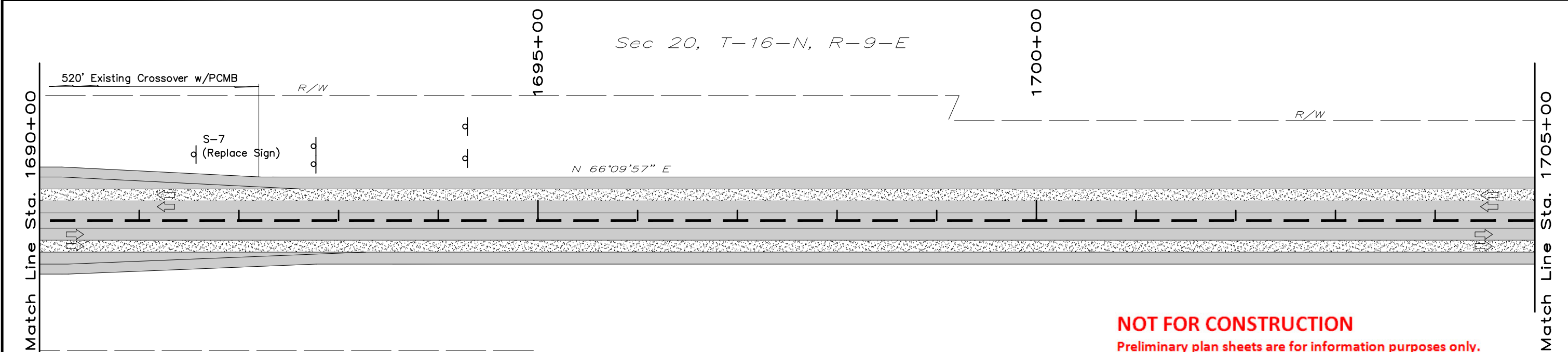
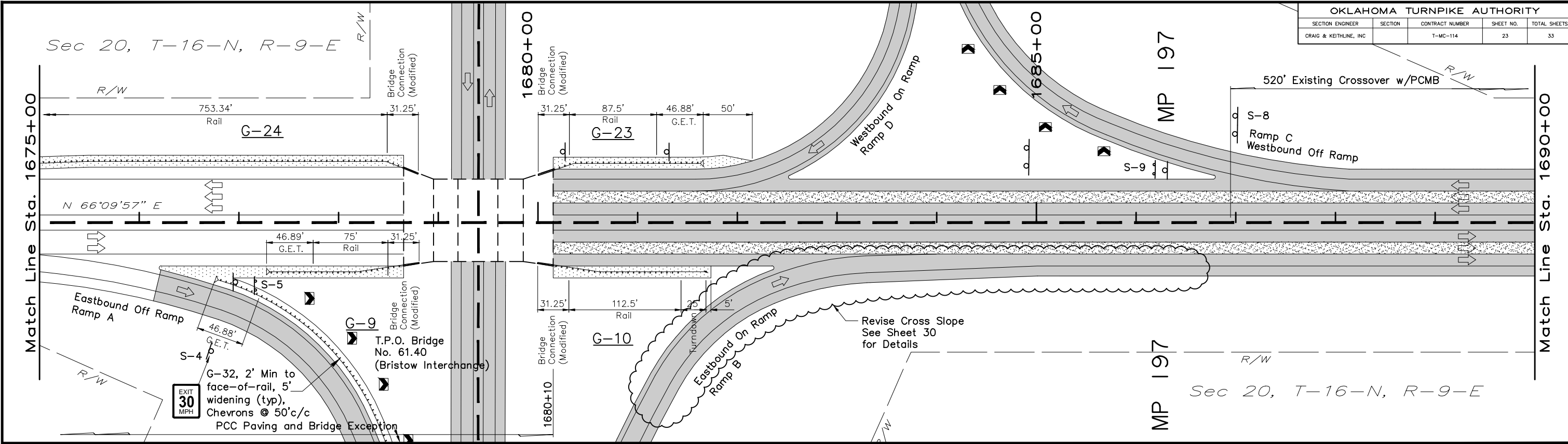


Note:
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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1645+00 TO STA. 1675+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 22 OF 33 SHEETS	

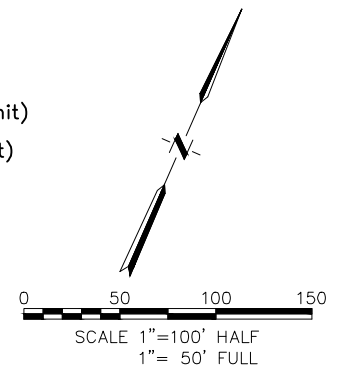
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Note:
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- LEGEND**
- Shallow Mill & Overlay
 - Deep Mill & Overlay
 - Full Depth Reconstruction
 - Guardrail Widening
 - Existing Median Barrier
 - Sign
 - Sign to be Replaced/Added
 - Guardrail (Proposed)
 - Guardrail (Type B Anchor Unit)
 - Guardrail (G.E.T. Anchor Unit)
 - Headwall / End Treatment
 - Existing Edge Drain Outlet
 - New Edge Drain Outlet
 - Cross Drain
 - Side Drain

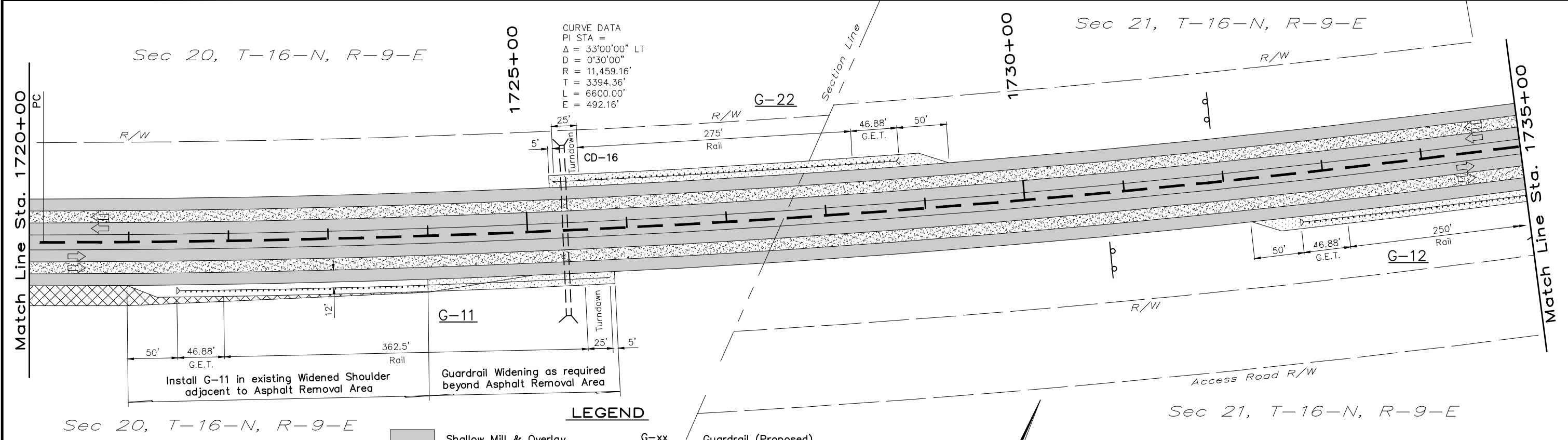
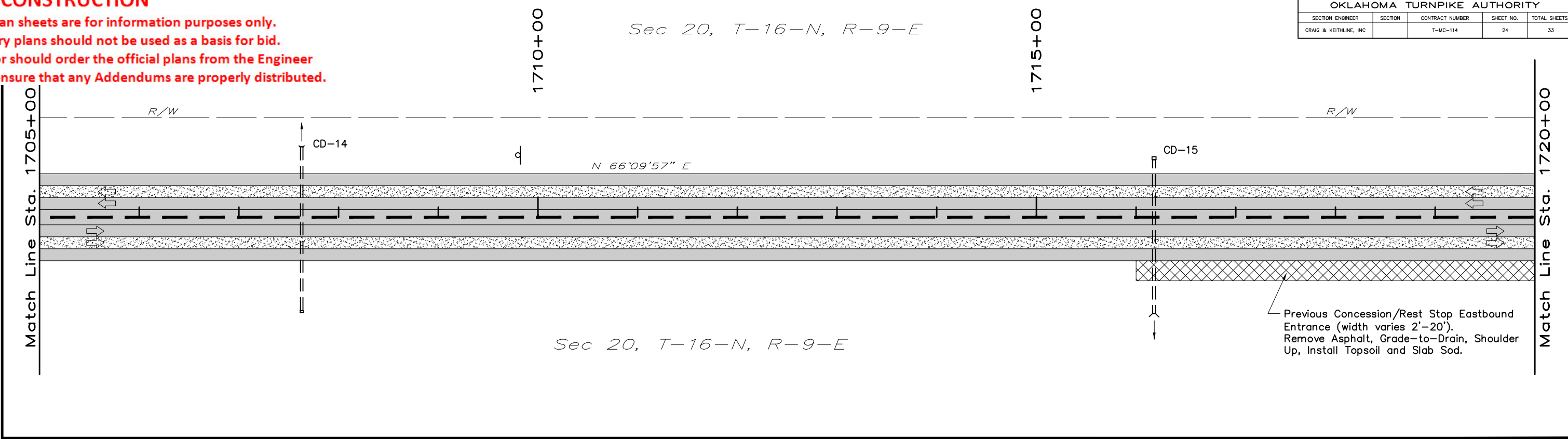


NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1675+00 TO STA. 1705+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 23 OF 33 SHEETS	

NOT FOR CONSTRUCTION

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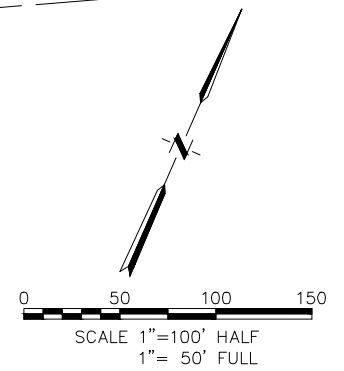
OKLAHOMA TURNPIKE AUTHORITY				
SECTION ENGINEER	SECTION	CONTRACT NUMBER	SHEET NO.	TOTAL SHEETS
CRAIG & KEITHLINE, INC.		T-MC-114	24	33



LEGEND

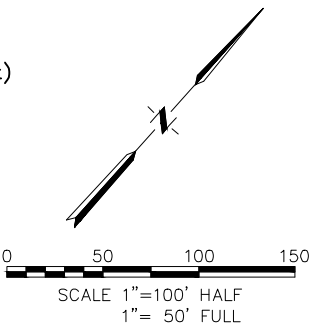
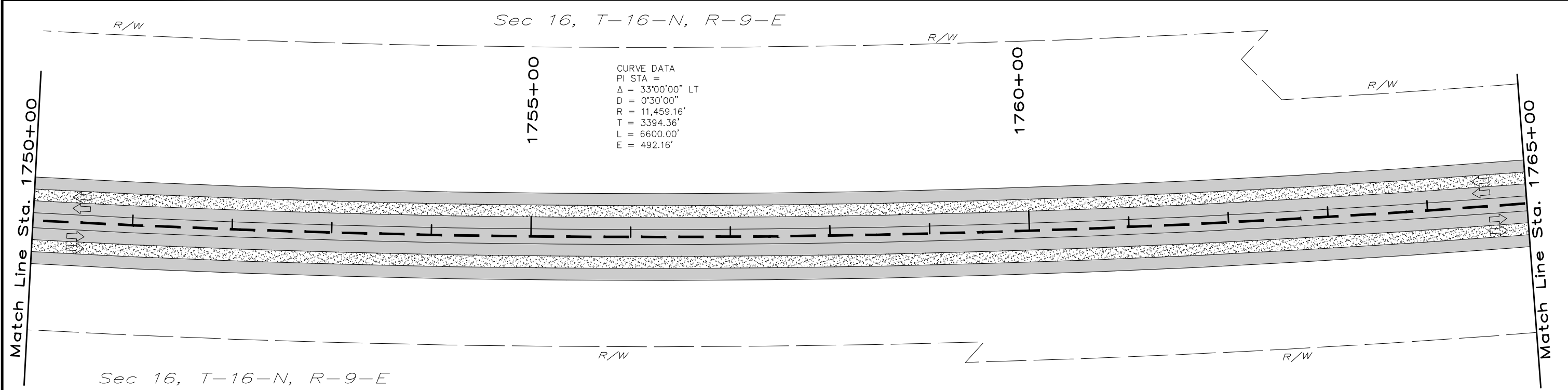
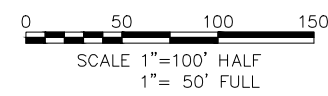
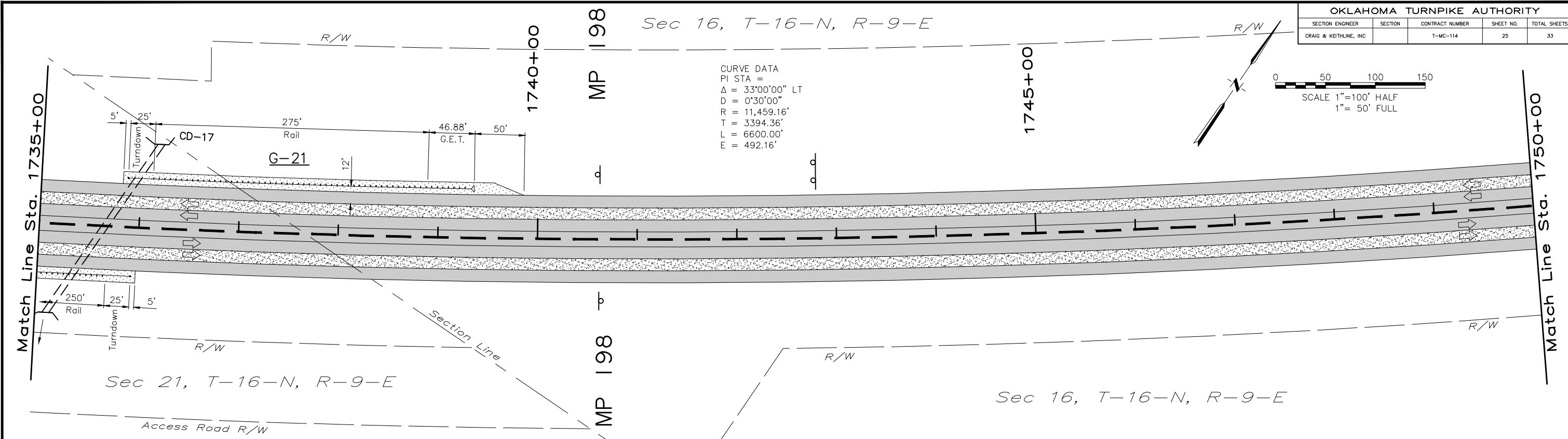
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|--|--------------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | G-XX Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | S-XX Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |

Note:
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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1705+00 TO STA. 1735+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 24 OF 33 SHEETS	

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LEGEND

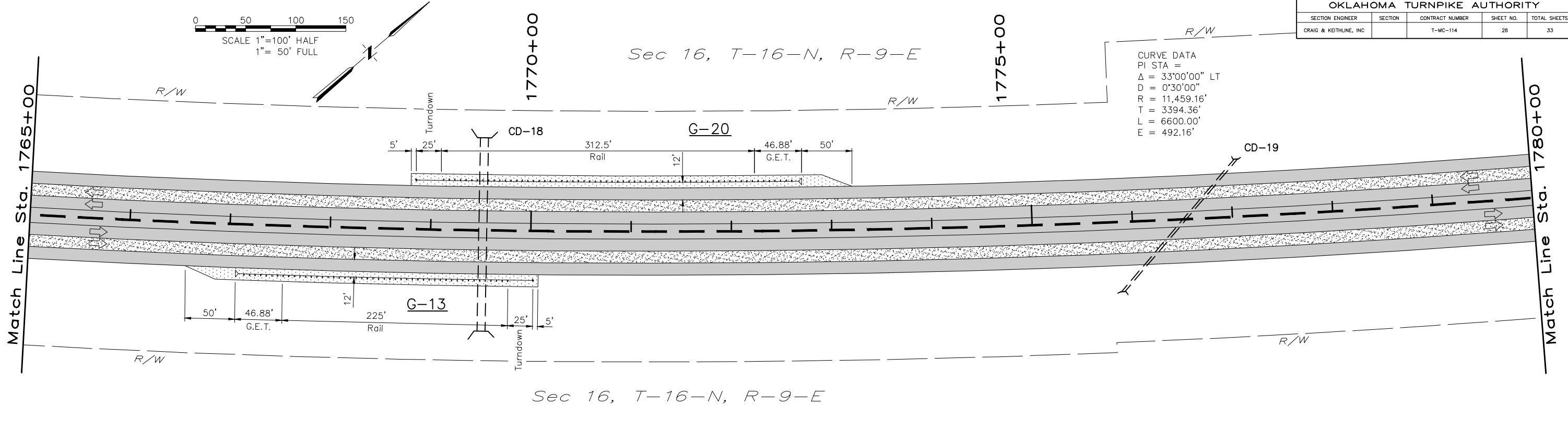
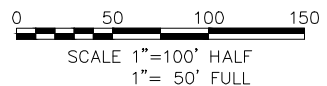
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|--|---------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |

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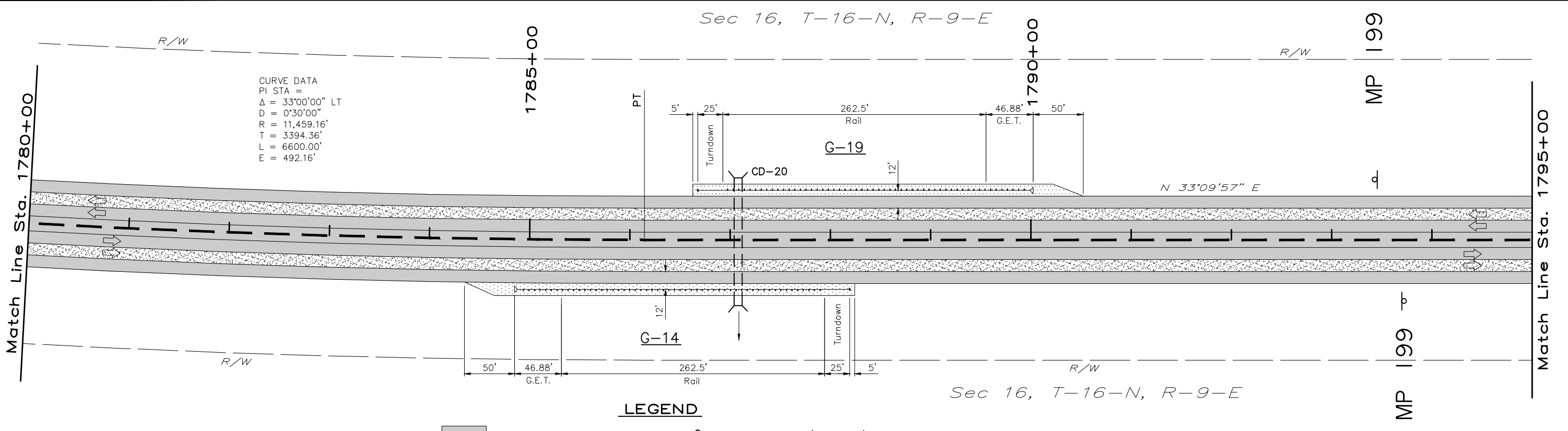
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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1735+00 TO STA. 1765+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 25 OF 33 SHEETS	

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CURVE DATA
 PI STA =
 $\Delta = 33^{\circ}00'00''$ LT
 $D = 0^{\circ}30'00''$
 $R = 11,459.16'$
 $T = 3394.36'$
 $L = 6600.00'$
 $E = 492.16'$



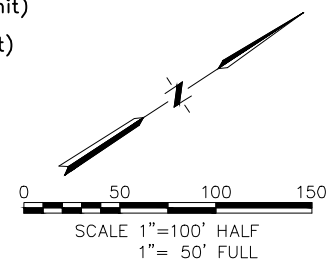
CURVE DATA
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 $\Delta = 33^{\circ}00'00''$ LT
 $D = 0^{\circ}30'00''$
 $R = 11,459.16'$
 $T = 3394.36'$
 $L = 6600.00'$
 $E = 492.16'$

LEGEND

- | | | | |
|--|---------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |

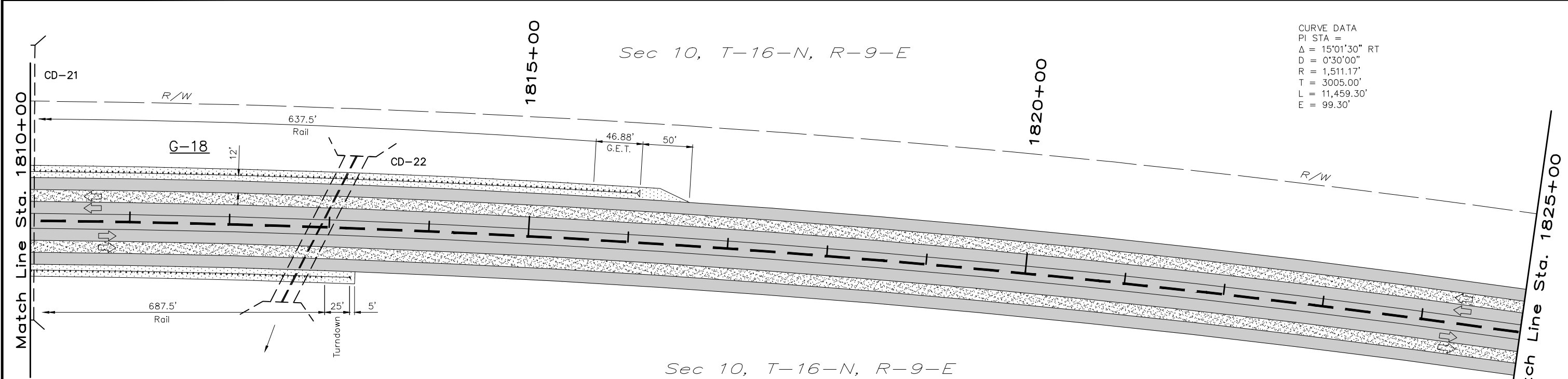
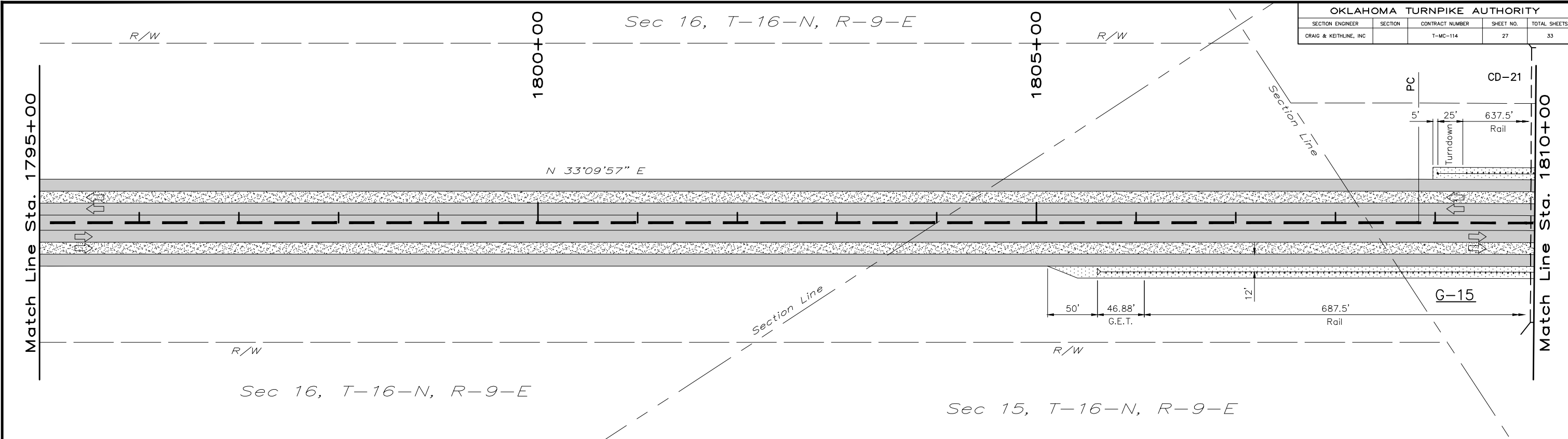
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NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1765+00 TO STA. 1795+00	
PROFILE SCALE HORIZONTAL			
VERTICAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 26 OF 33 SHEETS	

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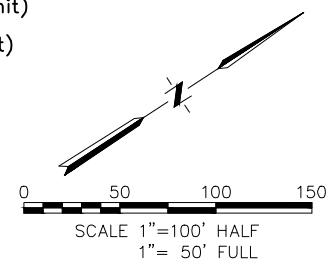
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 $D = 0^{\circ}30'00''$
 $R = 1,511.17'$
 $T = 3005.00'$
 $L = 11,459.30'$
 $E = 99.30'$

LEGEND

- | | | | |
|--|---------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |

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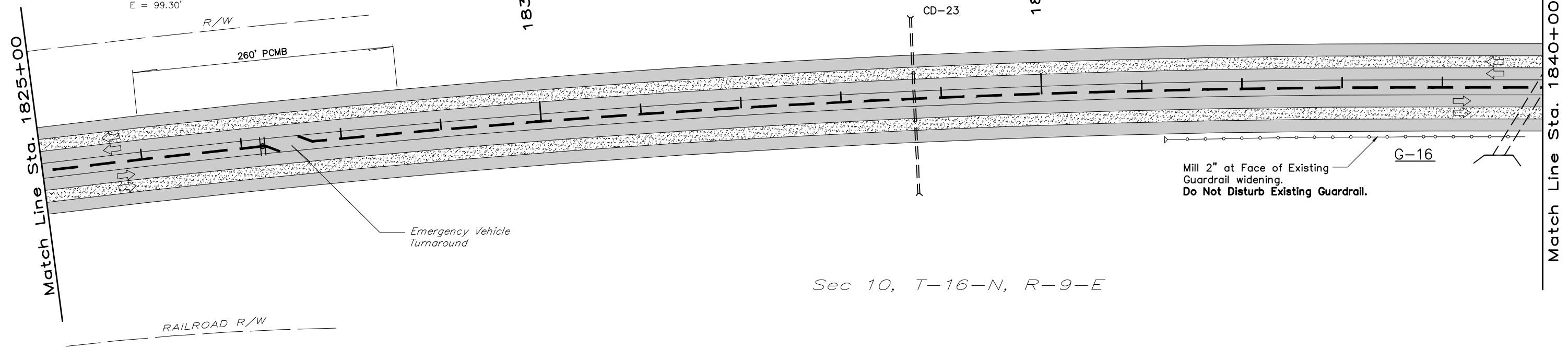


NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1795+00 TO STA. 1825+00	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 27 OF 33 SHEETS	

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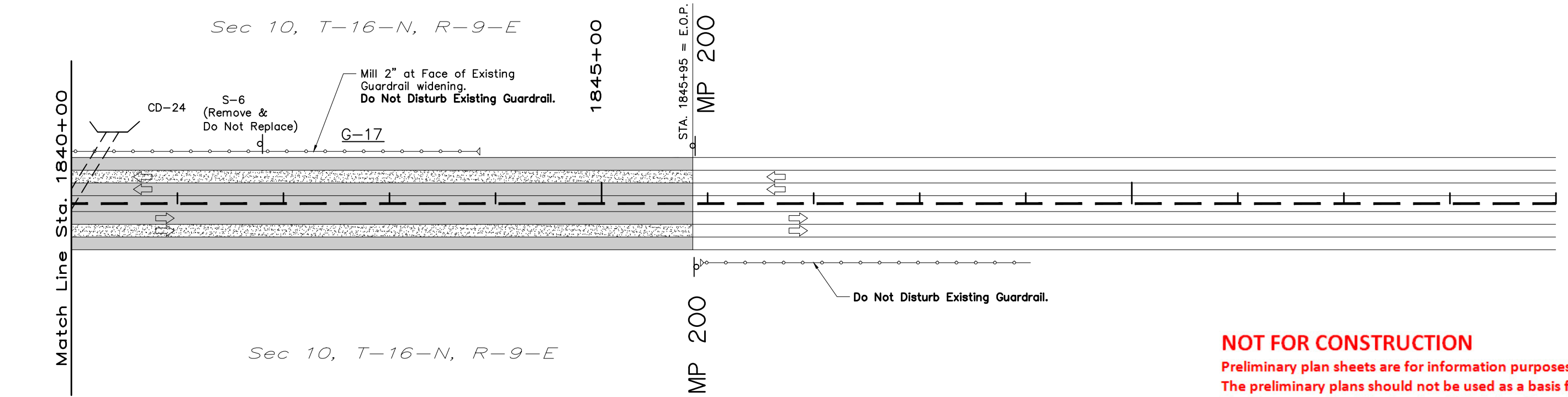
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 $\Delta = 15^{\circ}01'30''$ RT
 $D = 0^{\circ}30'00''$
 $R = 1,511.17'$
 $T = 3005.00'$
 $L = 11,459.30'$
 $E = 99.30'$

Sec 10, T-16-N, R-9-E



Sec 10, T-16-N, R-9-E

Sec 10, T-16-N, R-9-E



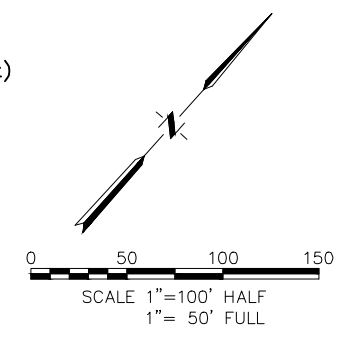
Sec 10, T-16-N, R-9-E

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LEGEND

- | | | | |
|--|---------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Reconstruction | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Existing Median Barrier | | Existing Edge Drain Outlet |
| | Sign | | New Edge Drain Outlet |
| | Sign to be Replaced/Added | | Cross Drain |
| | | | Side Drain |

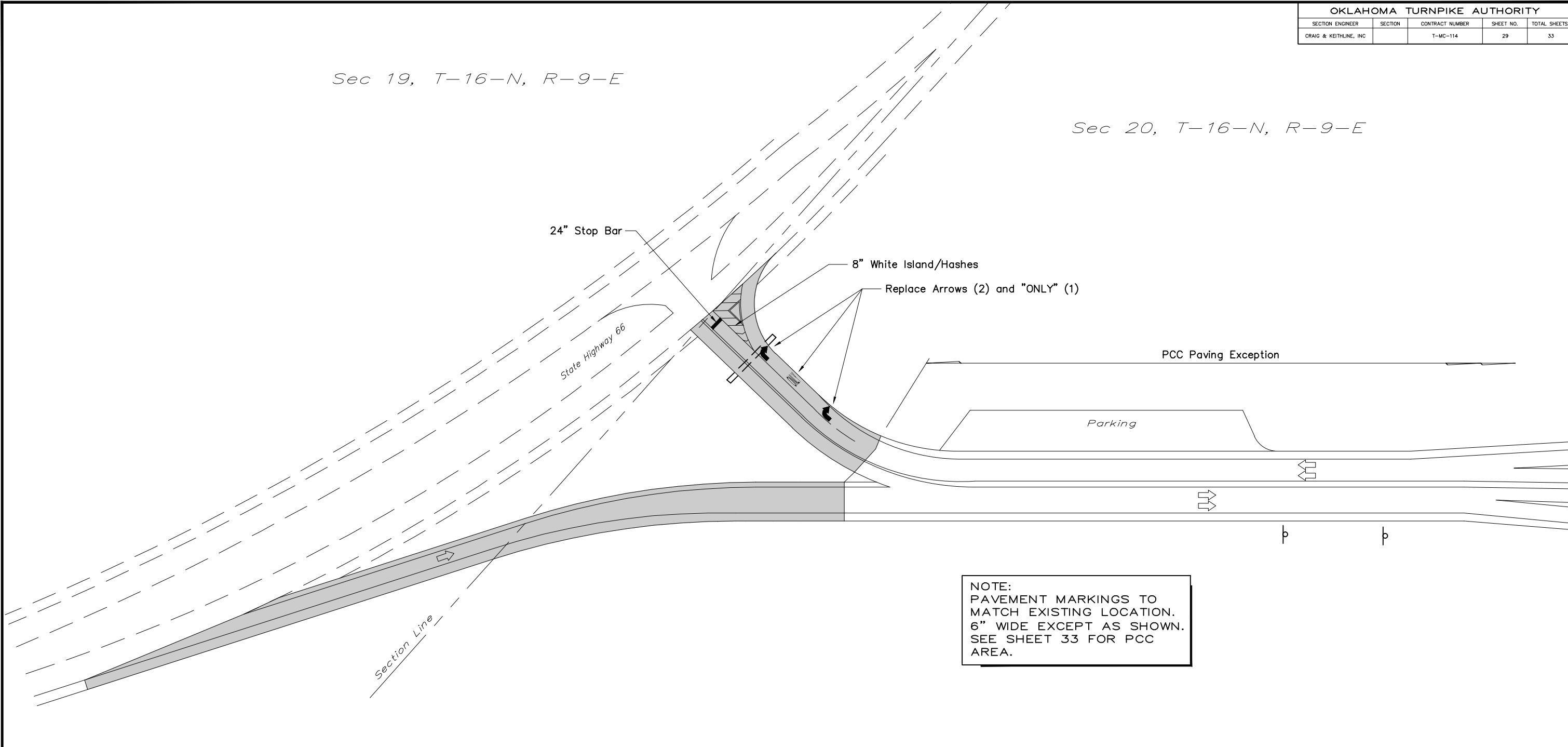
Note:
 Plan sheets are a schematic representation based on existing mileposts. Actual centerline measured length has been used for all quantity calculations. Delineator placement shall be adjusted at each existing milepost.



NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION STA. 1825+00 TO STA. 1845+95	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 28 OF 33 SHEETS	

Sec 19, T-16-N, R-9-E

Sec 20, T-16-N, R-9-E

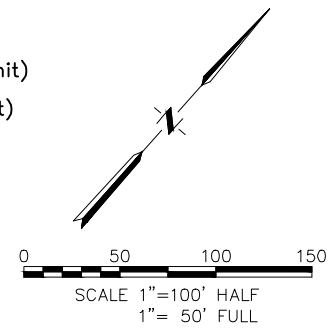


NOTE:
PAVEMENT MARKINGS TO
MATCH EXISTING LOCATION.
6" WIDE EXCEPT AS SHOWN.
SEE SHEET 33 FOR PCC
AREA.

NOT FOR CONSTRUCTION
Preliminary plan sheets are for information purposes only.
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of Record to ensure that any Addendums are properly distributed.

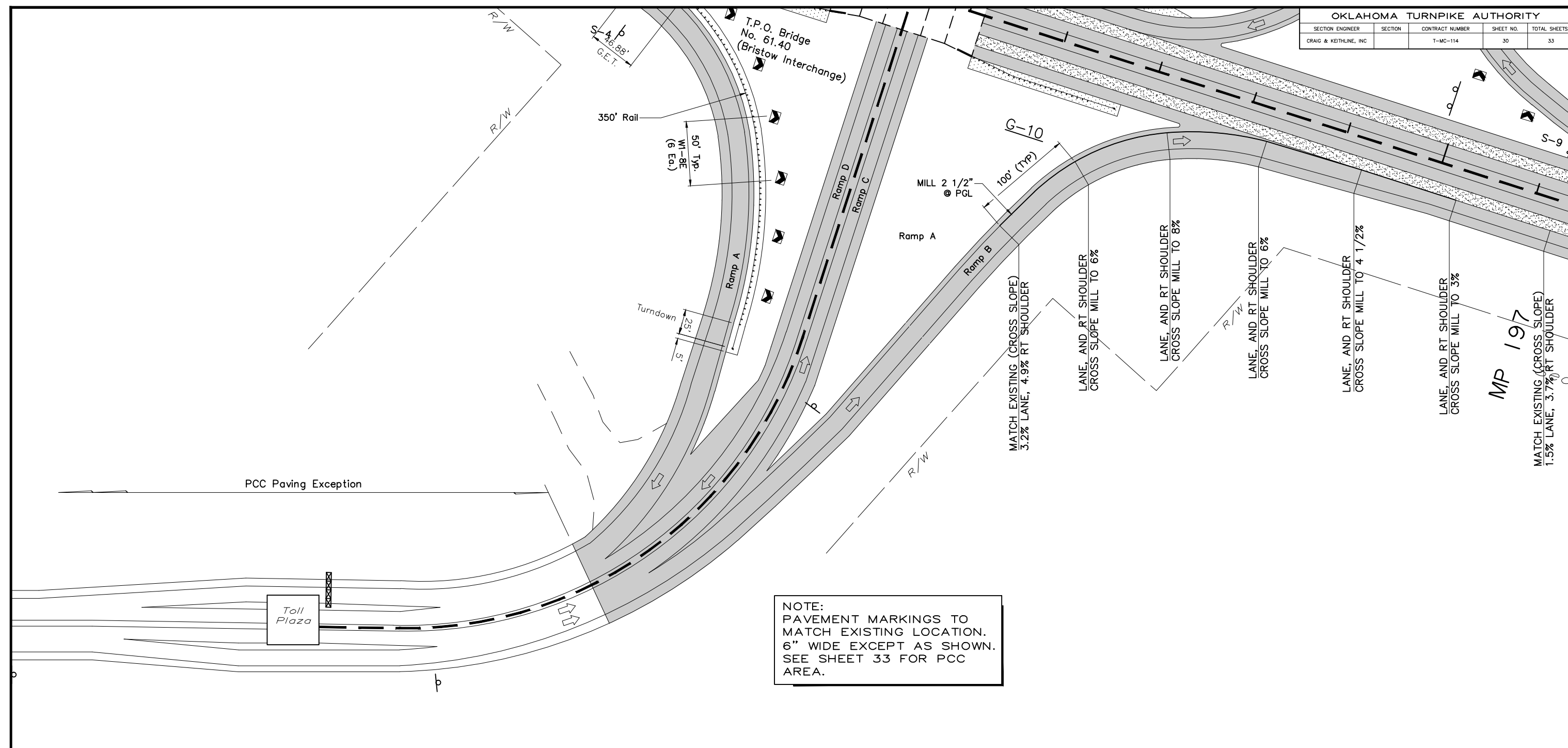
Note:
Plan sheets are a schematic representation based
on existing mileposts. Actual centerline measured
length has been used for all quantity calculations.
Delineator placement shall be adjusted at each
existing milepost.

LEGEND	
	Shallow Mill & Overlay
	Deep Mill & Overlay
	Full Depth Reconstruction
	Guardrail Widening
	Existing Median Barrier
	Sign
	Sign to be Replaced/Added
	Guardrail (Proposed)
	Guardrail (Type B Anchor Unit)
	Guardrail (G.E.T. Anchor Unit)
	Headwall / End Treatment
	Existing Edge Drain Outlet
	New Edge Drain Outlet
	Cross Drain
	Side Drain



NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION BRISTOW INTERCHANGE	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 29 OF 33 SHEETS	

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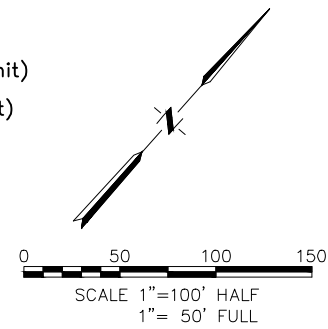
NOTE:
PAVEMENT MARKINGS TO
MATCH EXISTING LOCATION.
6" WIDE EXCEPT AS SHOWN.
SEE SHEET 33 FOR PCC
AREA.

Sec 20, T-16-N, R-9-E

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length has been used for all quantity calculations.
Delineator placement shall be adjusted at each
existing milepost.

- LEGEND**
- | | | | |
|--|------------------------|--|--------------------------------|
| | Shallow Mill & Overlay | | Guardrail (Proposed) |
| | Deep Mill & Overlay | | Guardrail (Type B Anchor Unit) |
| | Full Depth Patching | | Guardrail (G.E.T. Anchor Unit) |
| | Guardrail Widening | | Headwall / End Treatment |
| | Cross Drain | | Existing Edge Drain Outlet |
| | Side Drain | | New Edge Drain Outlet |
| | Sign | | Existing Median Barrier |

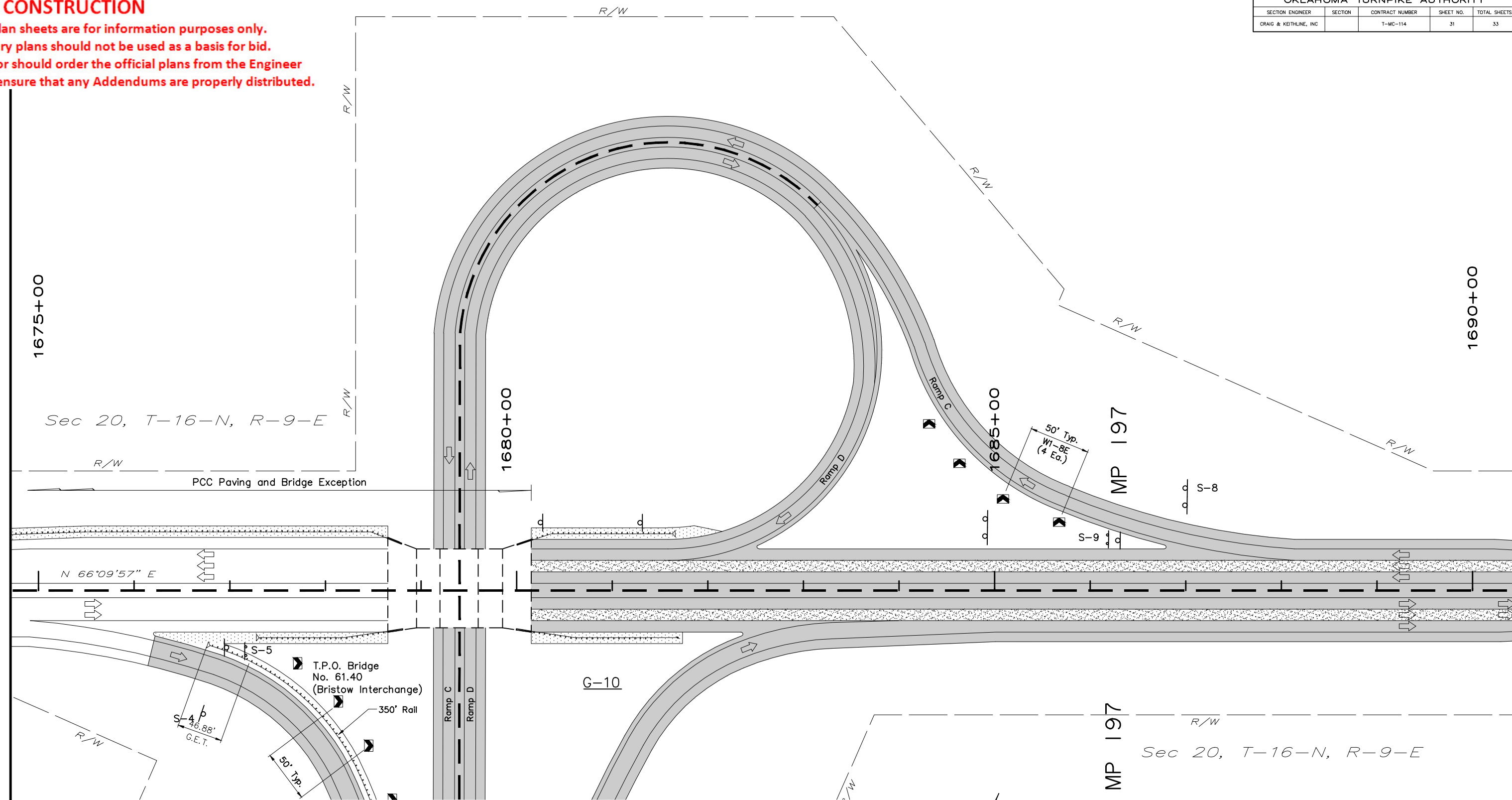


NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION BRISTOW INTERCHANGE	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 30 OF 33 SHEETS	

NOT FOR CONSTRUCTION

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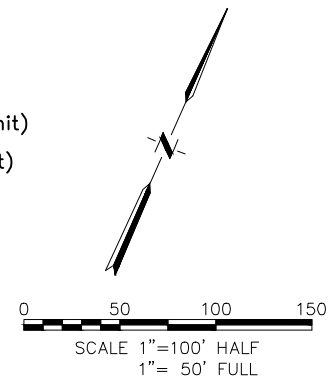
OKLAHOMA TURNPIKE AUTHORITY				
SECTION ENGINEER	SECTION	CONTRACT NUMBER	SHEET NO.	TOTAL SHEETS
CRAIG & KEITHLINE, INC.		T-MC-114	31	33



Note:
 Plan sheets are a schematic representation based on existing mileposts. Actual centerline measured length has been used for all quantity calculations. Delineator placement shall be adjusted at each existing milepost.

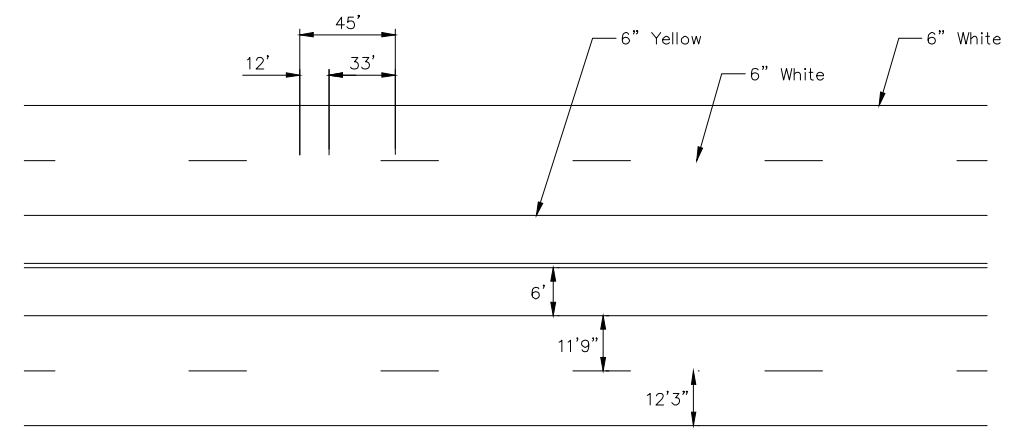
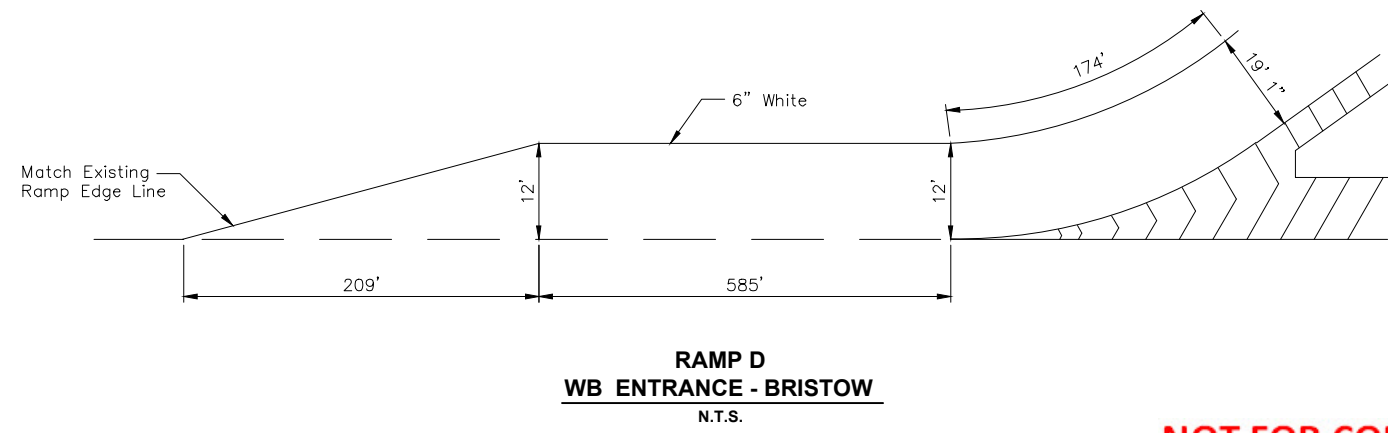
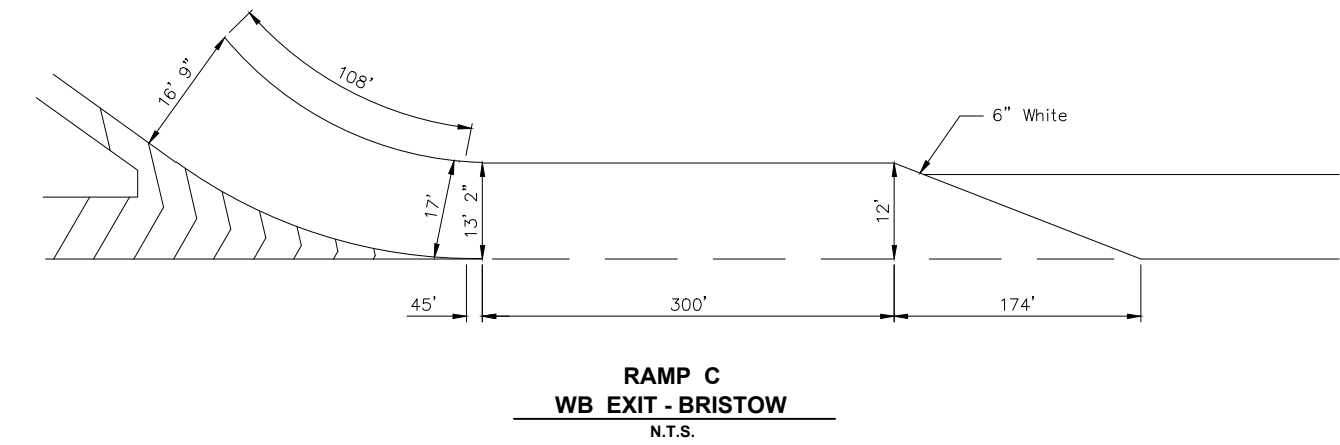
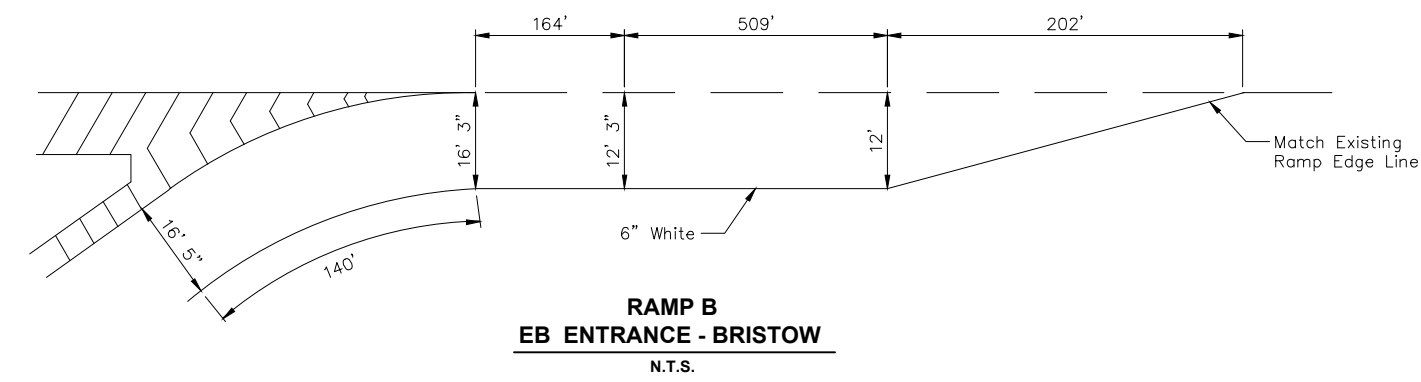
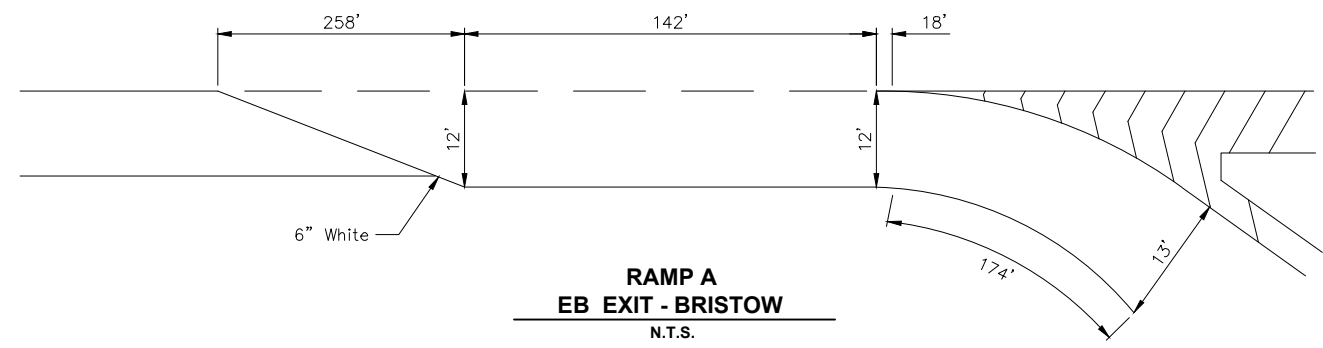
LEGEND

	Shallow Mill & Overlay		Guardrail (Proposed)
	Deep Mill & Overlay		Guardrail (Type B Anchor Unit)
	Full Depth Reconstruction		Guardrail (G.E.T. Anchor Unit)
	Guardrail Widening		Headwall / End Treatment
	Existing Median Barrier		Existing Edge Drain Outlet
	Sign		New Edge Drain Outlet
	Sign to be Replaced/Added		Cross Drain
			Side Drain



NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE 1" = 100' HALF 1" = 50' FULL		PAVEMENT REHABILITATION BRISTOW INTERCHANGE	
PROFILE SCALE HORIZONTAL		CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	
VERTICAL		SECTION	
DESIGNED	CKE	CONTRACT NO.	T-MC-114
DRAWN	CKD	DATE:	
CHECKED		SHEET 31 OF 33 SHEETS	

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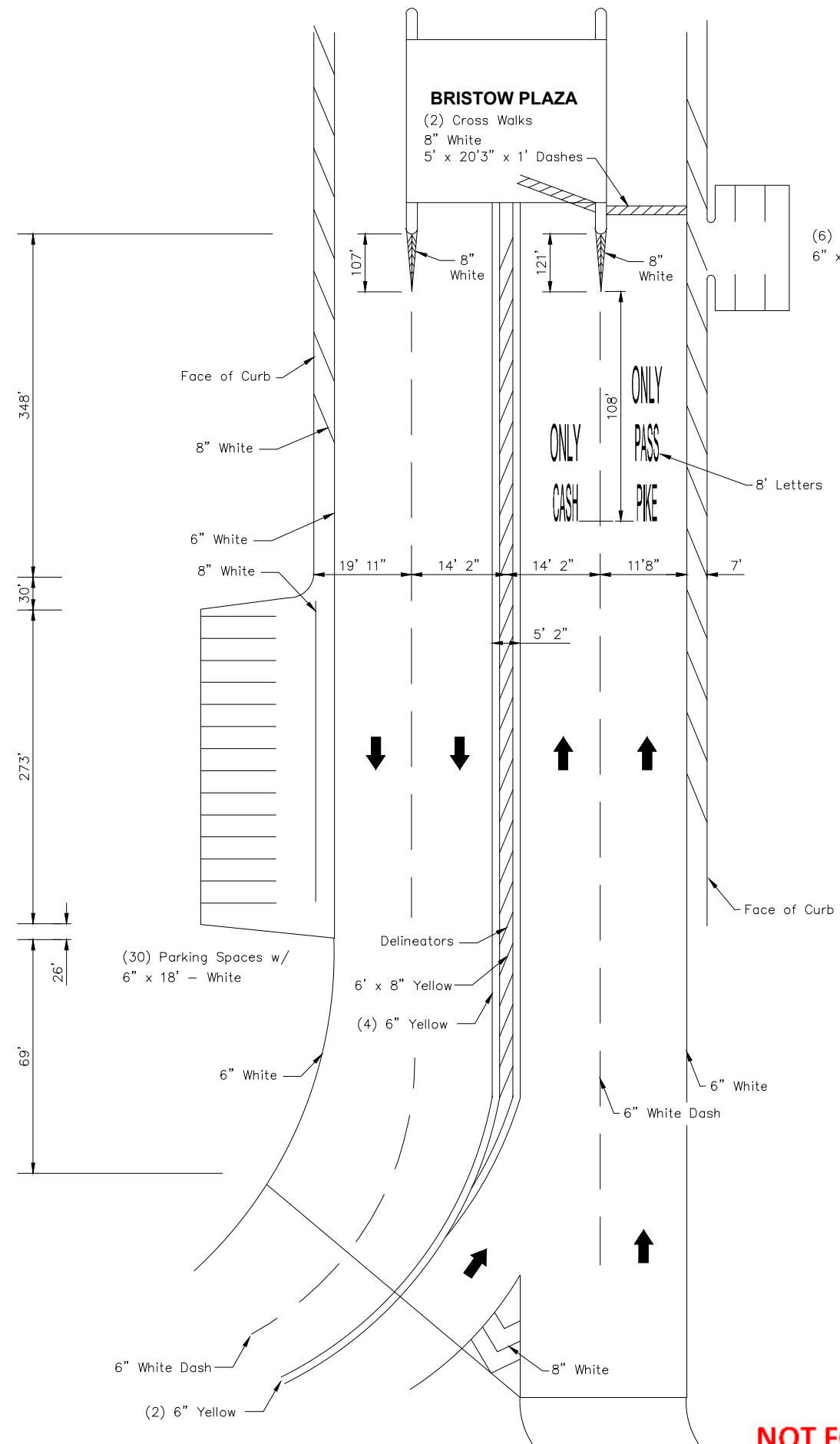


NOTE:
For Gore / Chevron Details
See Std. PM4-1-01



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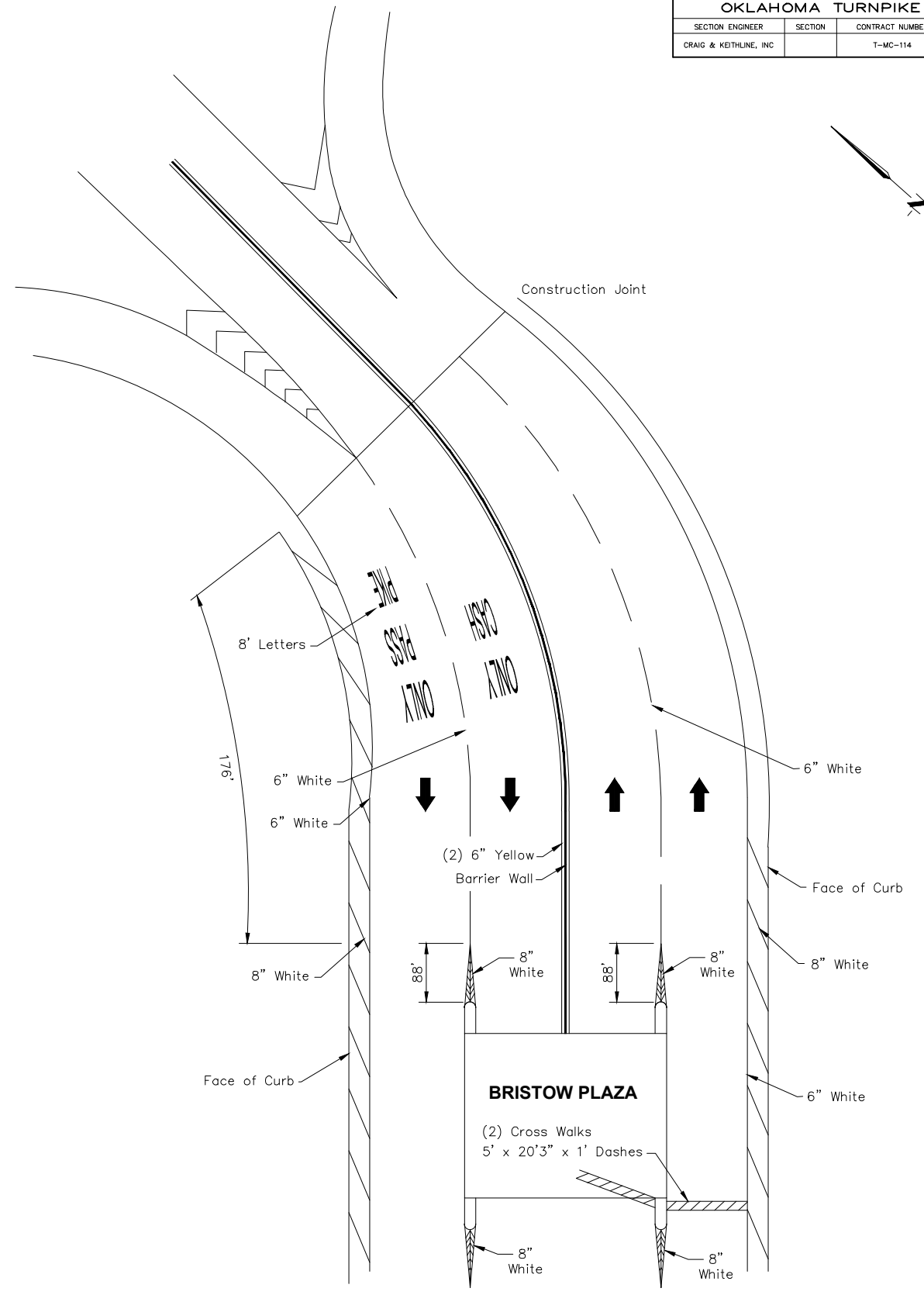
NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE	PAVEMENT MARKING PLAN MAINLINE & RAMPS		
PROFILE SCALE			
HORIZONTAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma	SECTION	
VERTICAL			
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 32 OF 33 SHEETS	



BRISTOW PLAZA - WEST
N.T.S.

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This Document is Preliminary in Nature and is Not a Final, Signed and Sealed Document



BRISTOW PLAZA - EAST
N.T.S.

NO.	REVISION	BY	DATE
OKLAHOMA TURNPIKE AUTHORITY TURNER TURNPIKE			
PLAN SCALE	PAVEMENT MARKING PLAN BRISTOW PLAZA		
PROFILE SCALE			
HORIZONTAL			
VERTICAL	CRAIG & KEITHLINE, INC. Tulsa, Oklahoma		SECTION
DESIGNED	CKE	CONTRACT NO. T-MC-114	
DRAWN	CKD	DATE:	
CHECKED		SHEET 33 OF 33 SHEETS	

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